

SECTION 2400  
STANDARD DETAILS FOR PAVING

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PAVEMENT DESIGN STANDARDS

1.

TRANSVERSE LIMITS OF PAVING SUBGRADE PREP SHALL EXTEND TO A MIN OF 1 FOOT BEYOND THE BACK OF CURB.
2.

FOR TRANSVERSE PAVEMENT STRUCTURE EXTENDING BELOW BOTTOM OF CURB:

A.

AGGREGATE BASE COURSE (ABC), TREATED ABC, TREATED SUBGRADE SOILS, AND ASPHALT CONCRETE (AC) STRUCTURE EXTENDING MORE THAN 1/2 INCH BELOW THE BOTTOM OF A CURB OR CURB & GUTTER SHALL EXTEND TRANSVERSELY UNDER AND BEHIND THE CURB OR CURB & GUTTER TO A MIN OF 1 FOOT BEYOND THE BACK OF CURB.

B.

SEE TABLE FOR LIFT MATERIAL REQUIREMENTS.
3.

CITY STANDARD PAVEMENT DESIGNS BASED ON AN R-VALUE ≥ AND MAXIMUM TRAFFIC VOLUMES DEFINED BELOW:

a.

LOCAL RESIDENTIAL STREETS (SEE STD. DWG 2405 A)  
ROADWAY PROVIDES ACCESS TO A MAXIMUM OF 50 RESIDENTIAL LOTS OR HAS A MAXIMUM AWDT OF 500.

LIFT	THICKNESS
AC SURFACE COURSE	1 1/2"
AC BASE COURSE	1 1/2"

b.

MAJOR LOCAL STREETS (SEE STD DWG 2405 B)  
ROADWAY TO HAVE A MAXIMUM AWDT OF 3000.

LIFT	THICKNESS
AC SURFACE COURSE	2"
AC BASE COURSE	2"

c.

ROADS CLASSIFIED ON THE LONG RANG MAJOR STREET PLAN REQUIRE A PAVEMENT DESIGN IN ACCORDANCE WITH SECTION 23 OF THE DEVELOPMENT PROCESS MANUAL
4.

THE PAVEMENT STRUCTURE SECTION SHALL BE SELECTED SUCH THAT THE LIFTS OF MATERIAL MODULE TO 1/2 INCH OF THE BOTTOM OF CURB AND COMPLY WITH MATERIAL LIMITS SPECIFIED BELOW. (SEE STD. DWGS 2407 & 2408)
5.

ALL PAVEMENT MATERIAL THAT EXTENDS MORE THAN 1/2 INCH BELOW THE BOTTOM OF THE CURB SHALL BE EXTENDED TO 1 FOOT BEYOND THE BACK OF CURB.

MATERIAL LIFT THICKNESS REQUIREMENTS

PAVEMENT CONSTRUCTION MATERIALS				
MATERIAL	COMPACTED LIFTS [1]		NOTES	CONSTRUCTION TOLERANCES [3]
	MINIMUM	MAXIMUM		
FILL	4"	8"	SEE SECTION 204	± 1 1/4" (0.10 FT)
SUBGRADE	4"	8"	SEE SECTION 301 FOR SUBGRADE DEPTH REQUIREMENTS	± 1 1/4" (0.10 FT)
AGGREGATE BASE COURSE (ABC)	4"	6"	SEE SECTION 302 FOR ABC CONSTRUCTION REQUIREMENTS	± 1/2" (0.04 FT)
BITUMINOUS TREATED BASE (BTB)	4"	6"	SEE SECTION 305 FOR BTB CONSTRUCTION REQUIREMENTS	± 1/2" (0.04 FT)
CONCRETE TREATED BASE (CTB)	4"	6"	SEE SECTION 307 FOR CTB CONSTRUCTION REQUIREMENTS	± 1/2" (0.04 FT)
ASPHALT CONCRETE (AC)			SEE SECTION 116 FOR AC CONSTRUCTION REQUIREMENTS	
TYPE A, SP-I	3"	4"		± 1/4" (0.02 FT)
TYPE B, SP-II	2"	3"		± 1/4" (0.02 FT)
TYPE C, SP-III	1 1/2"	2 1/2"		± 1/4" (0.02 FT)
TYPE D, SP-IV	1"	2"		± 1/4" (0.02 FT)
TREATED SOILS	4"	8"	SEE SECTION 304, 342 FOR CONSTRUCTION REQUIREMENTS	

[1]

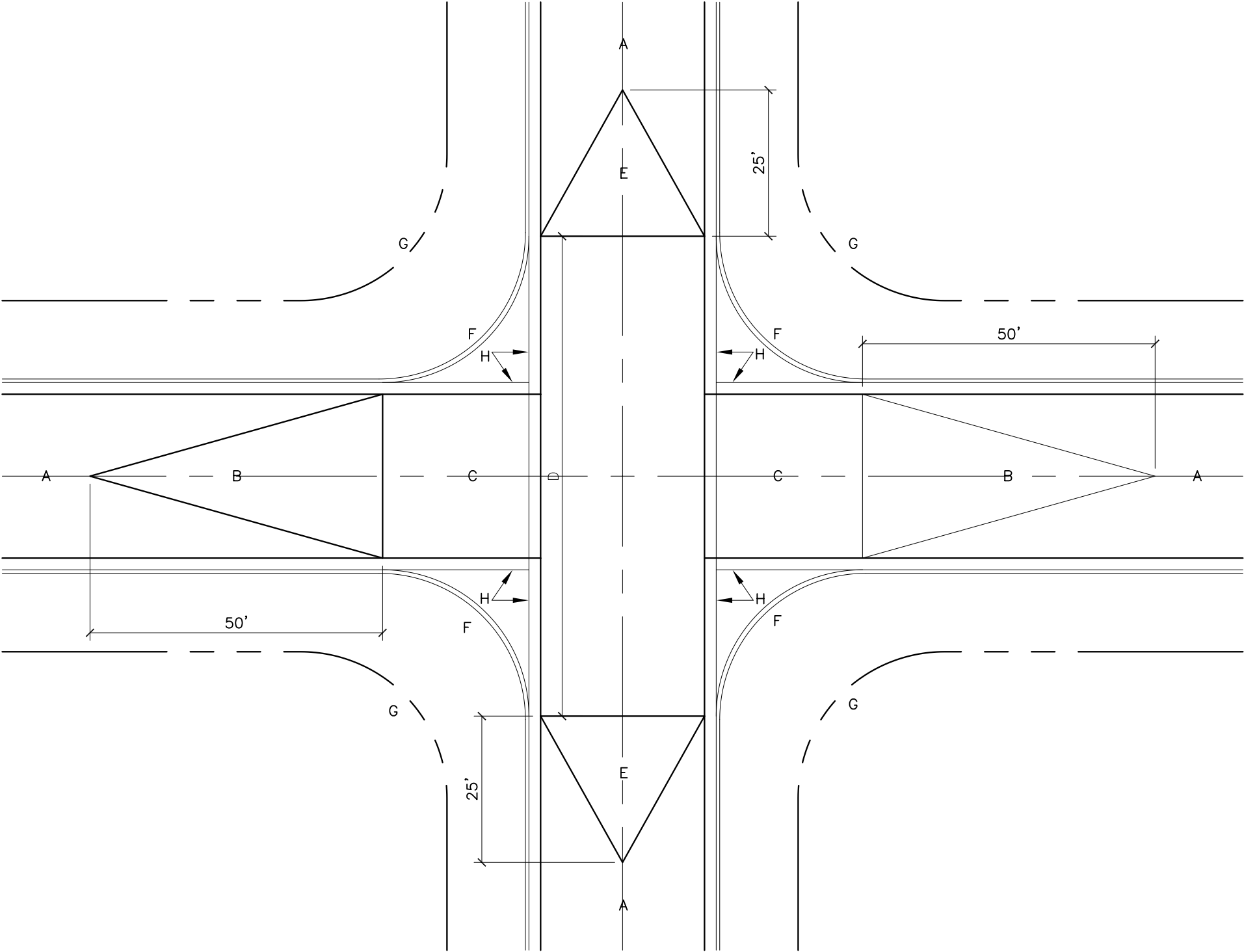
THE LIFT THICKNESS/DEPTH(S) FOR A PAVEMENT SECTION SHALL BE IDENTIFIED IN TYPICAL PAVEMENT SECTIONS ON A PROJECTS PLANS AND IN A PROJECT'S SPECIFICATIONS.

[2]

AGGREGATE BASE COURSE MAY BE USED IF PROPER DRAINAGE CAN BE PROVIDED.

[3]

MEASURED WITH A 10-FOOT STRAIGHT EDGE IN ANY DIRECTION.



**GENERAL NOTES:**

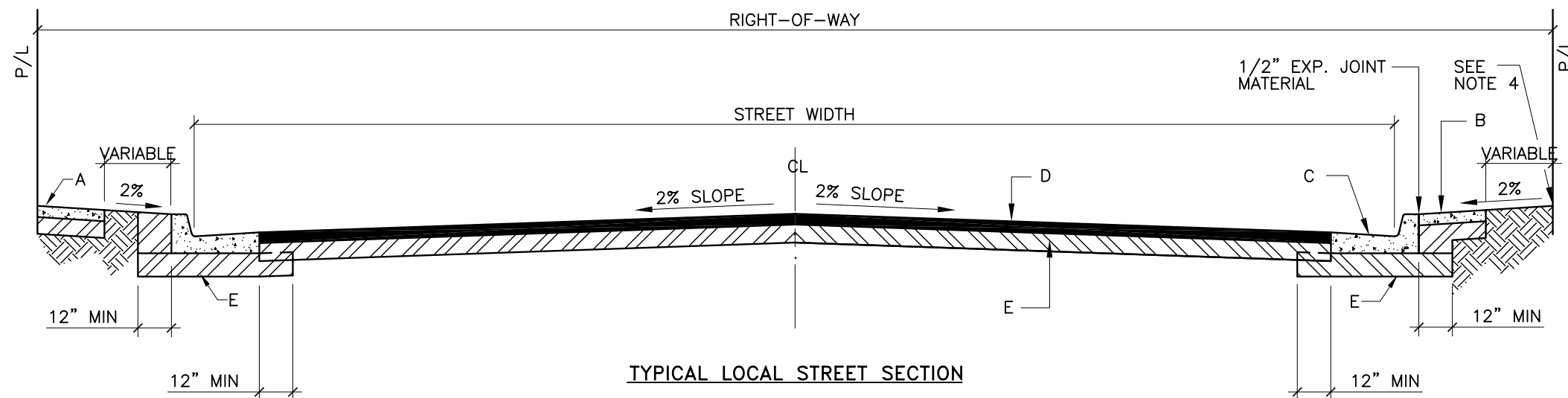
- 1. REDUCE NORMAL CROWN TO NO CROWN SECTION WHEN APPROACHING PERPENDICULAR TO VALLEY GUTTER.
- 2. REDUCE NORMAL CROWN TO HALF CROWN SECTION WHEN STREET IS PARALLEL TO VALLEY GUTTER.
- 3. FOR "T" INTERSECTIONS THE THROUGH STREET WILL RETAIN NORMAL CROWN & THE LEG OF THE "T" WILL REDUCE NORMAL CROWN TO NO CROWN SECTION WHEN APPROACHING PERPENDICULAR TO VALLEY GUTTER.
- 4. CONSTR. PLANS WILL DETAIL "T" INTERSECTION WHEN DRAINAGE FLOWS ACROSS THROUGH STREET OF INTERSECTION.
- 5. CONSTR. PLANS WILL SPECIFY RADIUS OF CURB RETURNS.

**CONSTRUCTION NOTES:**

- A. NORMAL 2% CROWN FOR RESIDENTIAL STREET.
- B. TRANSITION SECTION FROM FULL CROWN TO NO CROWN SECTION.
- C. NO CROWN SECTION.
- D. HALF CROWN SECTION.
- E. TRANSITION SECTION FROM FULL CROWN TO HALF CROWN SECTION.
- F. CURB RETURN.
- G. PROPERTY RETURN.
- H. FLOW LINE OF VALLEY GUTTER.

**TYPICAL RESIDENTIAL STREET INTERSECTION**  
**GRADING CONCEPT**

REVISIONS	CITY OF ALBUQUERQUE
1/91	PAVING TYPICAL RESIDENTIAL STREET INTERSECTION DWG. 2401      JANUARY 2003

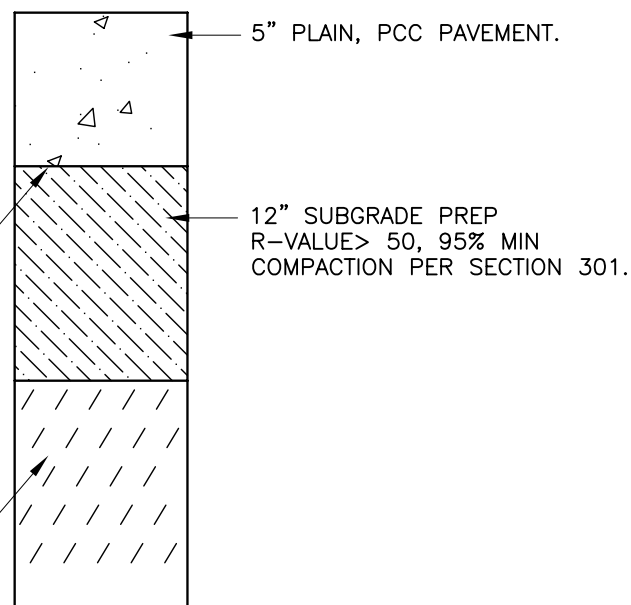


TYPICAL LOCAL STREET SECTION

FINISH SURFACE OR SUBGRADE SHALL BE MOISTURE CONTROLLED AT COMPACTION MOISTURE RANGE, AND/OR PRIME COAT APPLIED AS REQUIRED BY THE ENGINEER.

SEE SECTIONS 202, 204, AND 301 FOR CUT, FILL, AND SUBGRADE CONSTRUCTION REQUIREMENTS.

RIGID PAVEMENT SECTION

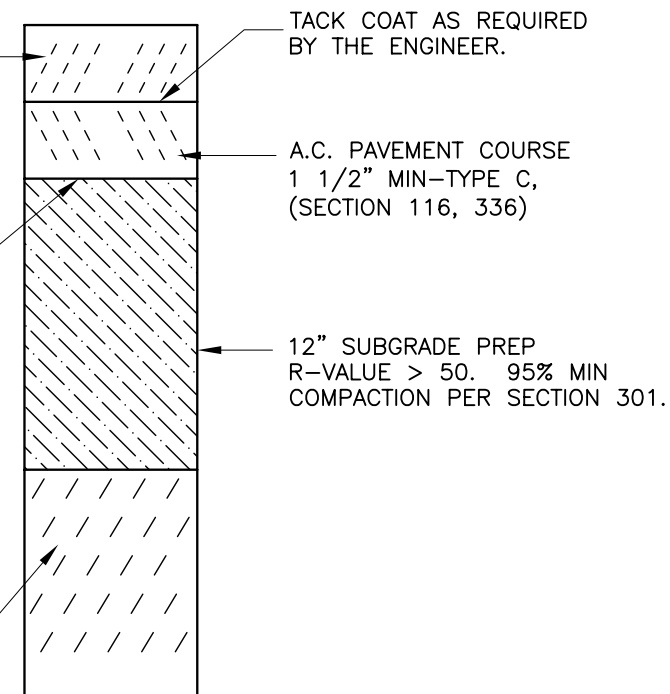


A.C. SURFACE COURSE 1 1/2" MIN-TYPE C, (SECTION 116, 336) PLACED AFTER ALL MANHOLE, VALVE COVERS/RINGS ARE SET TO GRADE.

FINISH SURFACE OF SUBGRADE SHALL BE MOISTURE CONTROLLED AT COMPACTION MOISTURE RANGE, AND/OR PRIME COAT APPLIED AS REQUIRED BY THE ENGINEER.

SEE SECTIONS 202, 204, AND 301 FOR CUT, FILL, AND SUBGRADE CONSTRUCTION REQUIREMENTS.

FLEXIBLE PAVEMENT SECTION



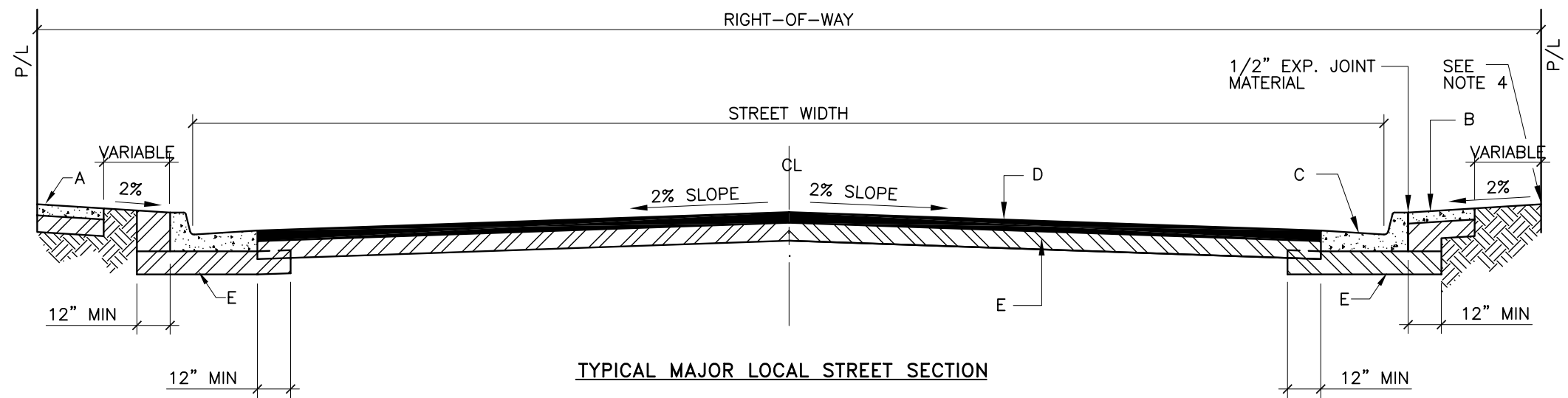
# GENERAL NOTES:

- CROWN ON STREET SHALL BE AS FOLLOWS:
  - 32' STREET = 4"
  - 40' STREET = 5"
  - LESS THAN 32' STREET, PAVEMENT SLOPE = 2%
- ALL SUBGRADE COMPACTION FOR C & G SHALL EXTEND 12" MIN ON EITHER SIDE OF C & G OR CURB SECTION.
- SUBGRADE PREPARATION UNDER SIDEWALK AND DRIVE PADS SHALL BE INCIDENTAL TO ITEM.
- FINISH GRADE AT PROPERTY LINE SHALL BE BASED ON A MIN 2% SLOPE FROM TOP OF CURB.
- ALL ASPHALT CONCRETE (AC) PAVEMENT SHALL COMPLY WITH SECTION 116.
- ALL PORTLAND CEMENT CONCRETE (PCC) PAVEMENT SHALL COMPLY WITH SECTION 101.
- IN ACCORDANCE WITH COA DPM THE FOLLOWING APPLIES UNLESS AUTHORIZED OTHERWISE BY THE CITY ENGINEER:
  - \* RESIDENTIAL STREETS SERVING 50 LOTS OR LESS SHALL BE DESIGNED AS LOCAL RESIDENTIAL STREETS.
  - \* RESIDENTIAL STREETS SERVING MORE THAN 50 LOTS WITH AN ANTICIPATED AWDT < 3000 SHALL BE DESIGNED AS MAJOR LOCAL STREETS.
- FOR SUBGRADE R-VALUE < 50, PAVEMENT SECTION SHALL BE DESIGNED IN ACCORDANCE WITH DPM CH. 23
- SUBGRADE PREPARATION SHALL BE PERFORMED AFTER ALL SUBSURFACE UTILITIES ARE CONSTRUCTED.

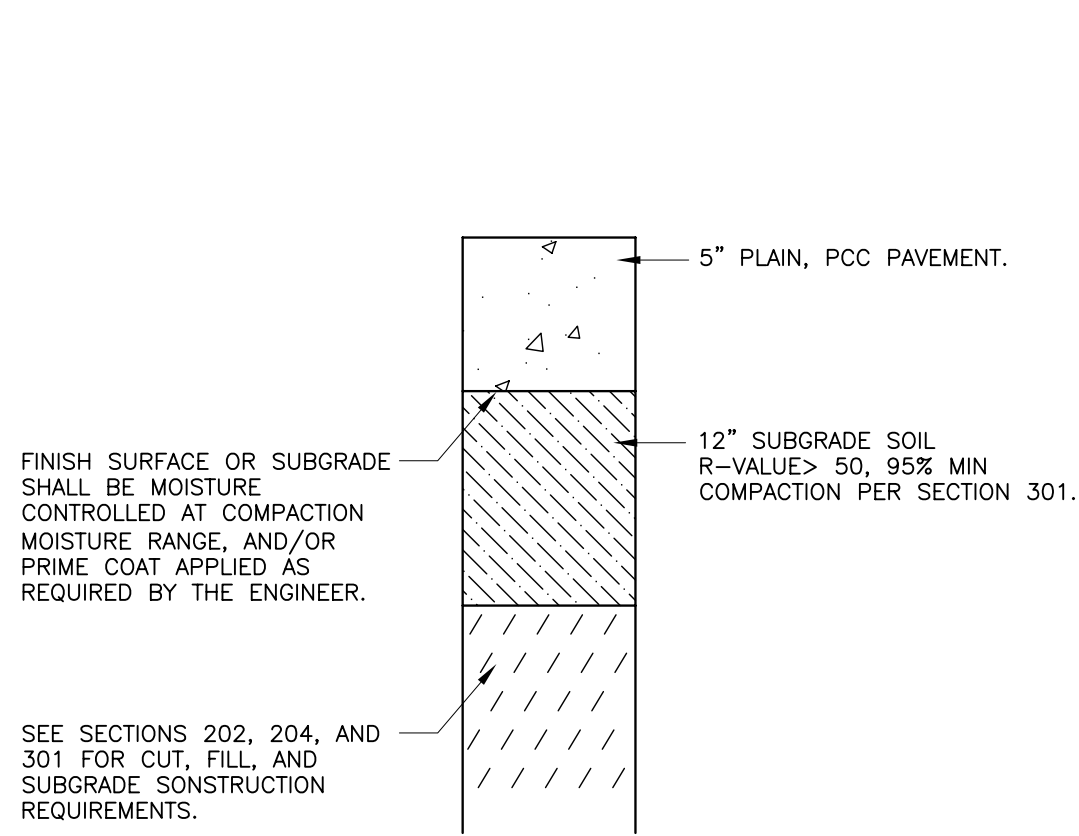
## CONSTRUCTION NOTES:

- SIDEWALK AT STANDARD SETBACK.
- SIDEWALK ADJACENT TO CURB. (NON-STANDARD, VARIANCE REQUIRED).
- STANDARD CURB AND GUTTER.
- ASPHALT CONCRETE (AC) OR PORTLAND CEMENT (PCC) PAVEMENT.
- 12" COMPACTED SUBGRADE PREP, 95% COMPACTION.

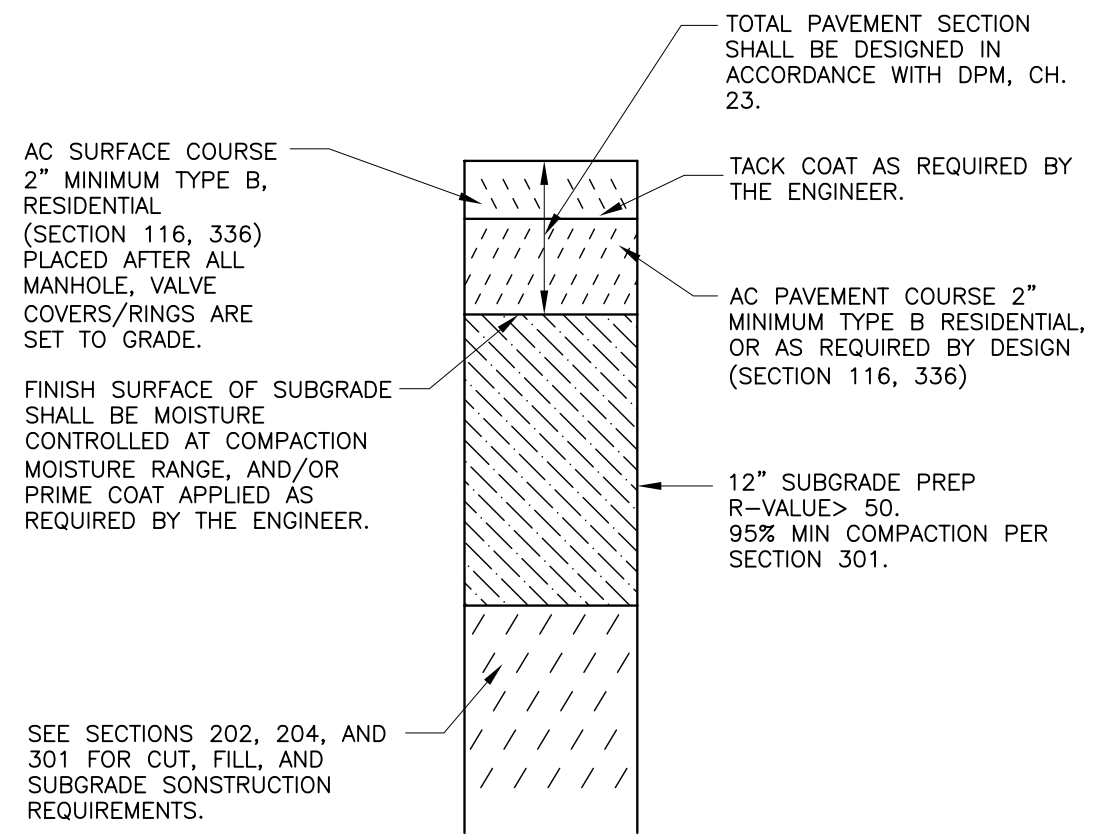
REVISIONS	CITY OF ALBUQUERQUE
1/91	PAVING
12/15/92	LOCAL - RESIDENTIAL
8/29/94	STREET SECTION
	DWG. 2405A JANUARY 2003



TYPICAL MAJOR LOCAL STREET SECTION



RIGID PAVEMENT SECTION



FLEXIBLE PAVEMENT SECTION

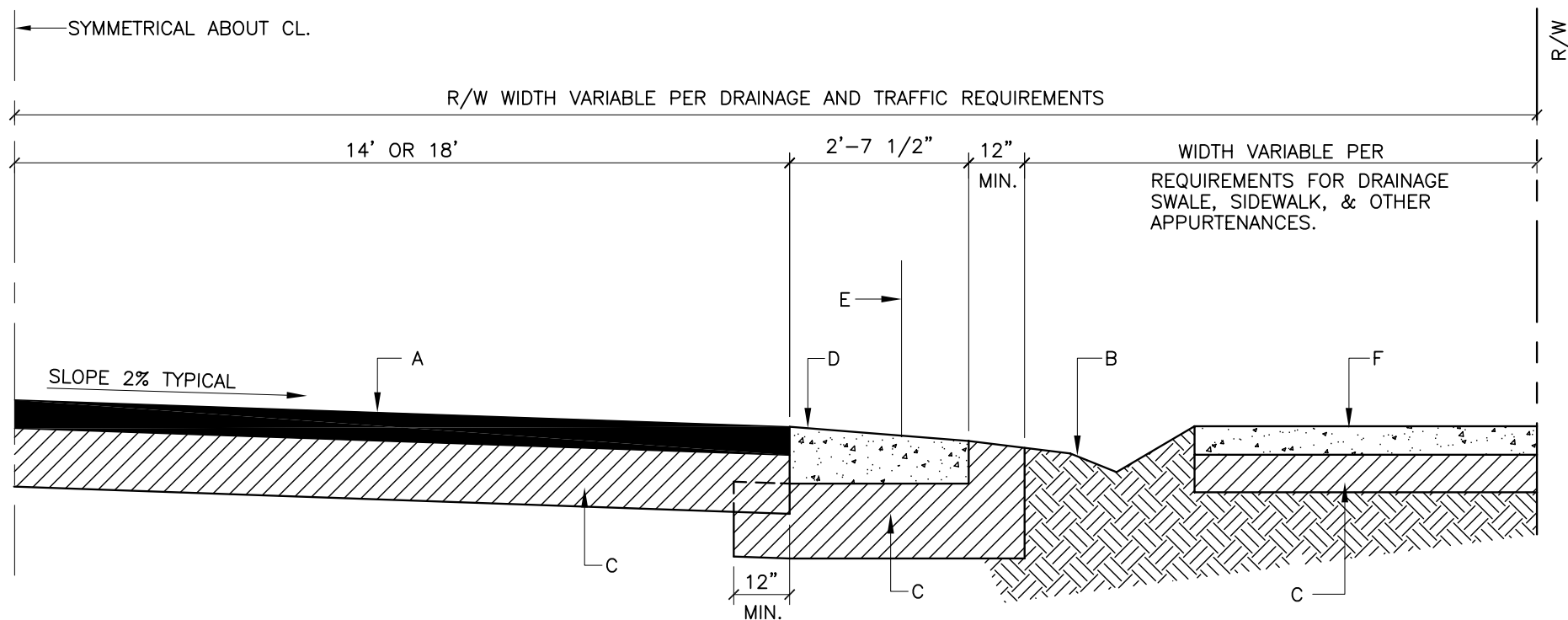
GENERAL NOTES:

- CROWN ON STREET SHALL BE AS FOLLOWS:
  - 32' STREET = 4"
  - 40' STREET = 5"
  - LESS THAN 32' STREET, PAVEMENT SLOPE = 2%
- ALL SUBGRADE COMPACTION FOR C & G SHALL EXTEND 12" MIN ON EITHER SIDE OF C & G OR CURB SECTION.
- SUBGRADE PREPARATION UNDER SIDEWALK AND DRIVE PADS SHALL BE INCLUDED WITH THE PARTICULAR ITEM.
- FINISH GRADE AT PROPERTY LINE SHALL BE BASED ON A MIN 2% SLOPE FROM TOP OF CURB.
- ALL ASPHALT CONCRETE (AC) PAVEMENT SHALL COMPLY WITH SECTION 116.
- ALL PORTLAND CEMENT CONCRETE (PCC) PAVEMENT SHALL COMPLY WITH SECTION 101.
- IN ACCORDANCE WITH COA DPM THE FOLLOWING APPLIES UNLESS AUTHORIZED OTHERWISE BY THE CITY ENGINEER:
  - \* RESIDENTIAL STREETS SERVING 50 LOTS OR LESS SHALL BE DESIGNED AS LOCAL RESIDENTIAL STREETS.
  - \* RESIDENTIAL STREETS SERVING MORE THAN 50 LOTS WITH AN ANTICIPATED AWDT < 3000 SHALL BE DESIGNED AS MAJOR LOCAL STREETS.
- FOR SUBGRADE R-VALUE < 50, PAVEMENT SECTION SHALL BE DESIGNED IN ACCORDANCE WITH DPM CH. 23.
- SUBGRADE PREPARATION SHALL BE PERFORMED AFTER ALL SUBSURFACE UTILITIES ARE CONSTRUCTED.

CONSTRUCTION NOTES:

- SIDEWALK AT STANDARD SETBACK.
- SIDEWALK ADJACENT TO CURB. (NON-STANDARD, VARIANCE REQUIRED).
- STANDARD CURB AND GUTTER.
- ASPHALT CONCRETE (AC) OR PORTLAND CEMENT (PCC) PAVEMENT.
- 12" COMPACTED SUBGRADE PREP, 95% COMPACTION.

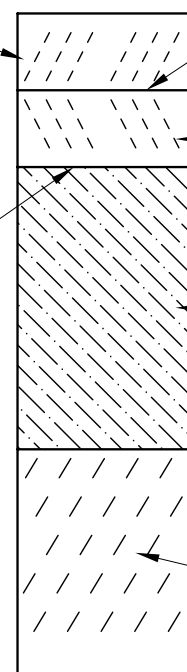
REVISIONS	CITY OF ALBUQUERQUE
1/91	PAVING
12/15/92	MAJOR LOCAL
8/29/94	STREET SECTION
	DWG. 2405B JANUARY 2003



**TYPICAL SECTION FOR  
32 FT. OR 40 FT. ESTATE TYPE STREET**

1-1/2" AC TYPICAL SURFACE COURSE—TYPE C PLACED AFTER ALL MANHOLE, VALVE COVERS/RINGS ARE SET TO GRADE. (SECTION 116,336).

FINISH SURFACE OF SUBGRADE SHALL BE MOISTURE CONTROLLED AT COMPACTION MOISTURE RANGE, AND/OR PRIME COAT APPLIED AS REQUIRED BY THE ENGINEER.



TACK COAT AS REQUIRED BY THE ENGINEER.

1-1/2" AC TYPE C PAVEMENT COURSE (SECTIONS 116, 336).

12" SUBGRADE SOIL R-VALUE > 50. 95% MIN COMPACTION PER SECTION 301.

SEE SECTIONS 202, 204, AND 301 FOR CUT, FILL, AND SUBGRADE CONSTRUCTION REQUIREMENTS.

**FLEXIBLE PAVEMENT SECTION**

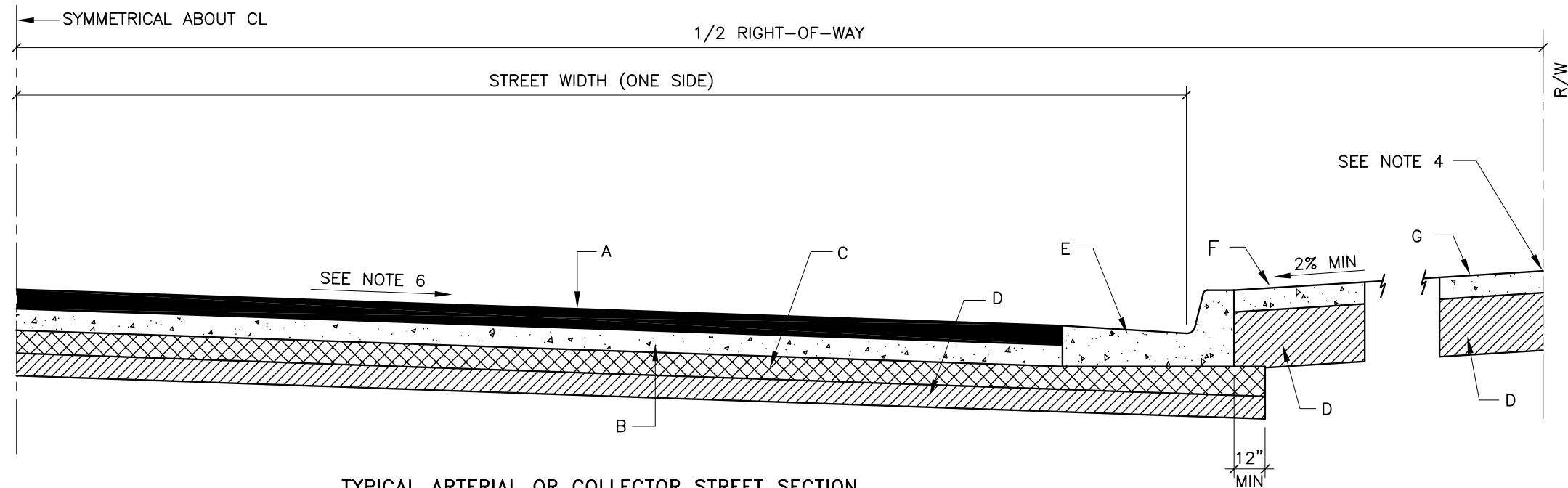
**GENERAL NOTES:**

1. ESTATE TYPE STREET SECTION TO BE USED ONLY WHEN PERMITTED IN THE APPROVED DRAINAGE PLANS.
2. RIGHT-OF-WAY REQUIREMENTS TO BE ESTABLISHED BY THE DRB. DESIGN OF SIDEWALK CONFIGURATION, DRAINAGE REQUIREMENTS & OTHER APPURTENANCE LOCATIONS SHALL BE APPROVED ON AN INDIVIDUAL SITE BASIS AND SHALL BE SHOWN ON THE PROJECT CONSTRUCTION PLANS.
3. ALL ASPHALT CONCRETE (AC) PAVEMENT SHALL COMPLY WITH SECTION 116.
4. IN ACCORDANCE WITH COA DPM THE FOLLOWING APPLIES UNLESS AUTHORIZED OTHERWISE BY THE CITY ENGINEER:
  - \* RESIDENTIAL STREETS SERVING 50 LOTS OR LESS SHALL BE DESIGNED AS LOCAL RESIDENTIAL STREETS.
  - \* RESIDENTIAL STREETS SERVING MORE THAN 50 LOTS AND WITH AWDT GREATER THAN 1000 SHALL BE DESIGNED AS MAJOR LOCAL STREETS.
5. FOR SUBGRADE R-VALUE <50, PAVEMENT SECTION SHALL BE DESIGNED IN ACCORDANCE WITH DPM, CH 23.
6. SUBGRADE PREPARATION SHALL BE PERFORMED AFTER ALL SUBSURFACE UTILITIES ARE CONSTRUCTED.

**CONSTRUCTION NOTES:**

- A. ASPHALT CONCRETE (AC) PAVEMENT.
- B. PROJECT-DESIGNED SWALE.
- C. COMPACTED SUBGRADE, 95% COMPACTION.
- D. MOUNTABLE CURB ESTATE TYPE.
- E. THEORETICAL FACE OF CURB OR FLOWLINE.
- F. SIDEWALK

REVISIONS	CITY OF ALBUQUERQUE
1/91	LOCAL RESIDENTIAL PAVING
12/15/92	ESTATE TYPE STREETS
8/29/94	
	DWG. 2406 JANUARY 2003

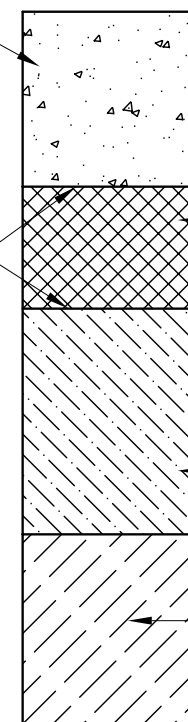


**TYPICAL ARTERIAL OR COLLECTOR STREET SECTION  
WITHOUT MEDIAN**

PLAIN/DOWELLED TRANSVERSE JOINTED PORTLAND CEMENT CONCRETE PAVEMENT SECTION DESIGNED IN ACCORDANCE WITH COA DPM, VOL. 2, SECTION 23, TRANSPORTATION DESIGN.

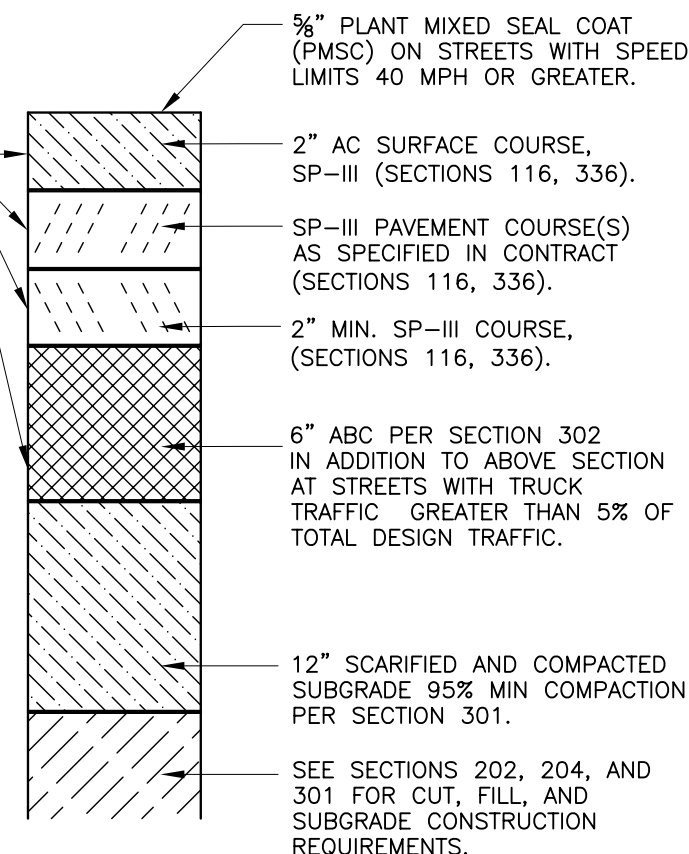
FINISH SURFACE OR SUBGRADE AGGREGATE BASE COURSE, AND OR CEMENT TREATED BASE SHALL BE MOISTURE CONTROLLED AT COMPACTION MOISTURE RANGE, AND PRIME COAT AS REQUIRED BY THE ENGINEER UNTIL NEXT/FINAL SURFACING COMPLETED.

SUBGRADE PREPARATION SHALL BE PERFORMED AFTER ALL SUBSURFACE R/W UTILITIES CONSTRUCTION COMPLETED.



**RIGID PAVEMENT SECTION**

PAVEMENT SECTION DESIGNED IN ACCORDANCE WITH COA DPM, VOL. 2, SECTION 23, STREET DESIGN.



**FLEXIBLE PAVEMENT SECTION**

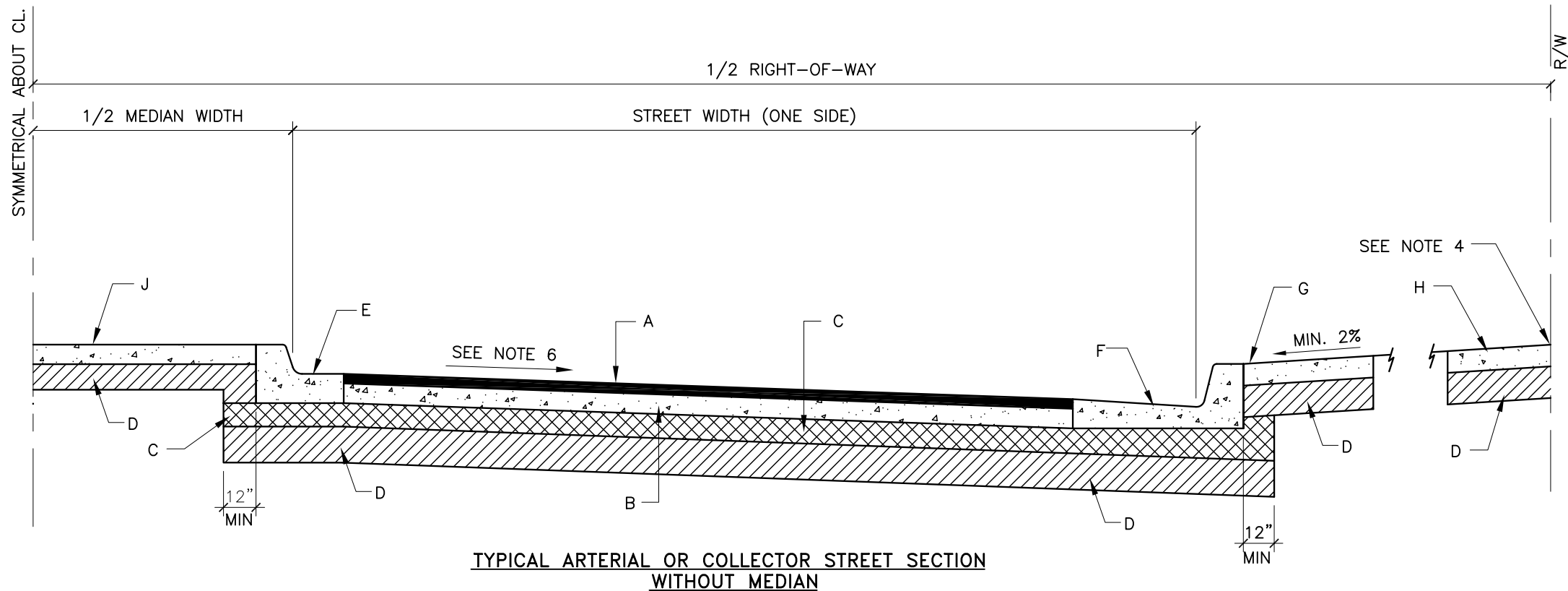
**GENERAL NOTES:**

1. STRUCTURAL THICKNESS OR PAVEMENT COMPONENTS WILL BE PER PAVEMENT DESIGN.
2. ALL SUBGRADE COMPACTION FOR C & G SHALL EXTEND 12\"
3. SUBGRADE PREPARATION UNDER SIDEWALK AND DRIVE PADS SHALL BE INCLUDED WITH THE PARTICULAR ITEM.
4. FINISH GRADE AT PROPERTY LINE SHALL BE BASED ON A MIN 2% SLOPE FROM TOP OF CURB.
5. SLOPE EASEMENT REQUIREMENTS WILL BE SHOWN ON PROJECT CONSTRUCTION PLANS.
6. TRANSVERSE SLOPE FOR PAVEMENT SHALL BE 2% TYPICAL.
7. GRADES AND ELEVATIONS SHALL BE MET BY SURFACE COURSE WITH PLANT MIX SEAL PLACED AS AN OVERLAY.
8. PLANT MIX SEAL SHALL BE PLACES ABOVE THE TOE OF THE GUTTER.
9. ALL ASPHALT CONCRETE (AC) PAVEMENT SHALL COMPLY WITH SECTION 116.
10. ALL PORTLAND CEMENT CONCRETE (PCC) PAVEMENT SHALL COMPLY WITH SECTION 101.

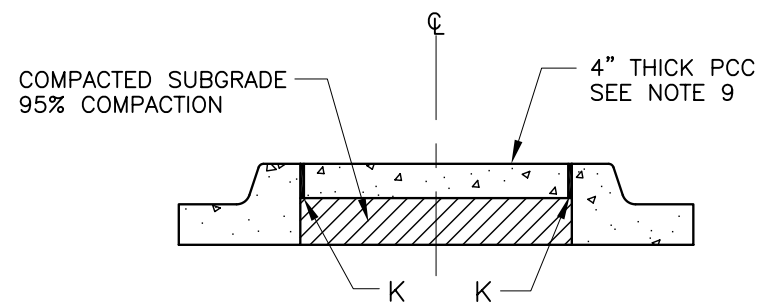
**CONSTRUCTION NOTES:**

- A. ASPHALT CONCRETE SURFACE COURSE.
- B. ASPHALT CONCRETE (AC) PAVEMENT.
- C. 6\"
- D. 12\"
- E. CURB & GUTTER STANDARD.
- F. SIDEWALK ADJACENT TO CURB (NON-STANDARD, VARIANCE REQUIRED).
- G. SIDEWALK AT STANDARD SETBACK.

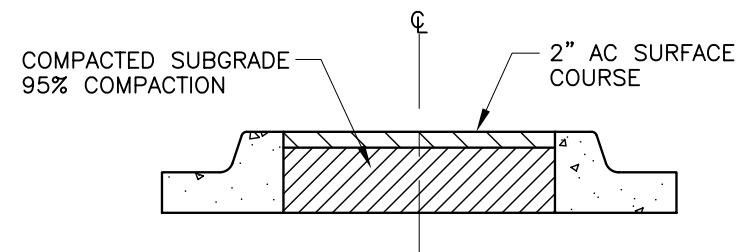
REVISIONS	CITY OF ALBUQUERQUE
1/91	PAVING
12/15/92	ART./COLL. ST. SECTIONS
8/29/94	WITHOUT MEDIAN
	DWG. 2407 JANUARY 2003



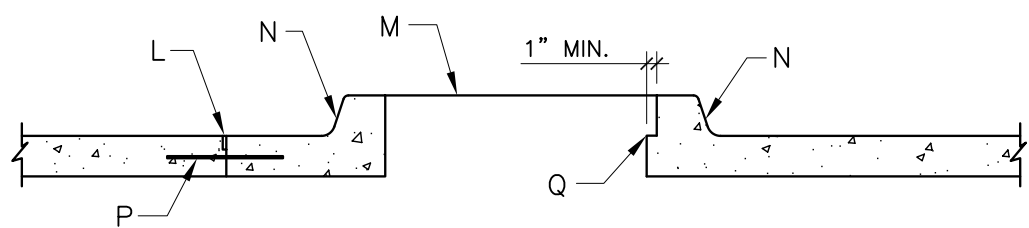
**TYPICAL ARTERIAL OR COLLECTOR STREET SECTION  
WITHOUT MEDIAN**



**CONCRETE MEDIAN PAVING SECTION  
(WIDTH VARIES)**

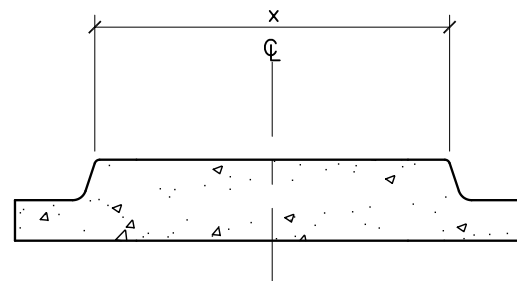


**BITUMINOUS MEDIAN PAVING SECTION  
(WIDTH VARIES)**



**CAST CURB & GUTTER**

**INTEGRAL CAST OR EXTRUDED  
PINNED CURB**



**CAST INTEGRAL MEDIAN  
WHERE x > 4 FT**

**GENERAL NOTES:**

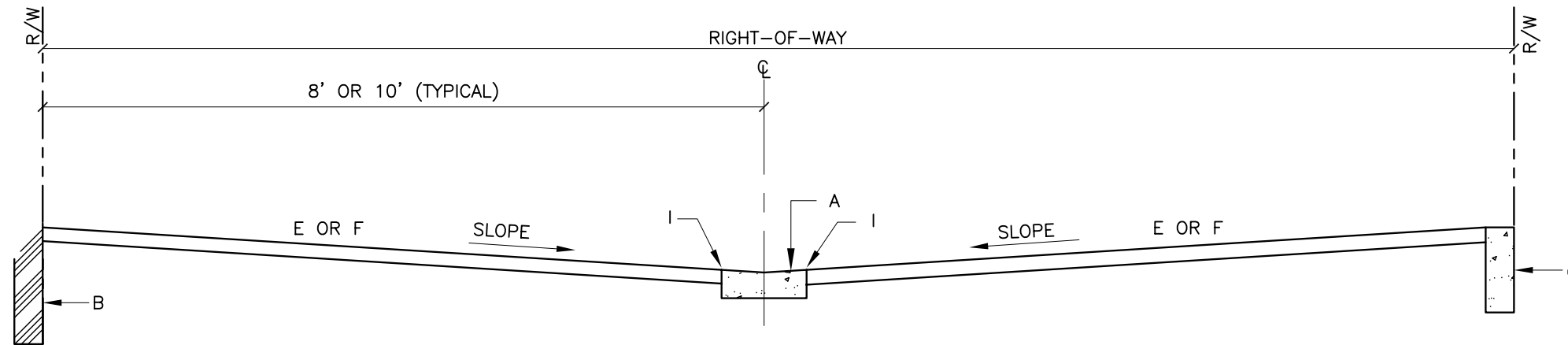
1. STRUCTURAL THICKNESS OR PAVEMENT COMPONENTS WILL BE PER PAVEMENT DESIGN.
2. ALL SUBGRADE COMPACTION FOR C & G SHALL EXTEND 12" MIN ON EITHER SIDE OF C & G OR CURB SECTION.
3. SUBGRADE PREPARATION UNDER SIDEWALK AND DRIVE PADS SHALL BE INCLUDED WITH THE PARTICULAR ITEM.
4. FINISH GRADE AT PROPERTY LINE SHALL BE BASED ON A MIN 2% SLOPE FROM TOP OF CURB.
5. SLOPE EASEMENT REQUIREMENTS WILL BE SHOWN ON PROJECT CONSTRUCTION PLANS.
6. TRANSVERSE SLOPE FOR PAVEMENT SHALL BE 2% TYPICAL.
7. PAVEMENT FINISH GRADES AND ELEVATIONS SHALL BE MET BY ASPHALT CONCRETE (AC) SURFACE COURSE.
8. PLANT MIX SEAL SHALL BE PLACES ABOVE THE TOE OF THE GUTTER.
9. PORTLAND CEMENT CONCRETE (PCC) MEDIAN PAVEMENT SHALL BE TEXTURED CONCRETE RUNNING BOND PATTERN TRANSVERSE TO CENTERLINE COLOR AS SPECIFIED.
10. SEE STANDARD DWG. 2407 FOR ARTERIAL/COLLECTOR, FLEXIBLE OR RIGID PAVEMENT SECTION.

**CONSTRUCTION NOTES:**

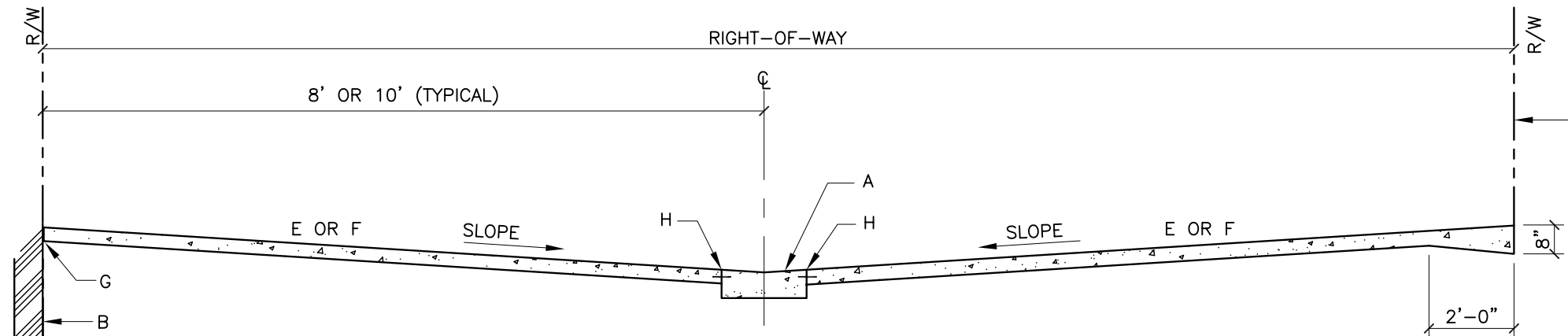
- A. ASPHALT CONCRETE (AC) SURFACE COURSE.
- B. ASPHALT CONCRETE (AC) PAVEMENT.
- C. COMPACTED BASE.
- D. COMPACTED SUBGRADE, 95% MIN.
- E. CURB & GUTTER MEDIAN.
- F. CURB & GUTTER STANDARD.
- G. SIDEWALK ADJACENT TO CURB (NON-STANDARD, VARIANCE REQUIRED).
- H. SIDEWALK AT STANDARD SETBACK.
- J. MEDIAN.
- K. 1/2" EXPANSION JOINT MATERIAL.
- L. SAW & SEAL JOINT PER STD. DETAIL.
- M. TYPICAL MEDIAN PAVING (SEE DETAIL).
- N. SEAL JOINT TO TOP OF CURB.
- P. #4 X 30" TIE BAR @ 2'-0" O.C. EXTENSION NOT REQUIRED AT INTEGRAL.
- Q. EXTENSION NOT REQUIRED AT INTEGRAL CAST CURB.

REVISIONS	CITY OF ALBUQUERQUE
9/91	PAVING
12/15/92	ART./COLL. ST. SECTIONS
8/29/94	WITH MEDIAN
	DWG. 2408 JANUARY 2003





**ASPHALT CONCRETE  
ALLEY SECTION**



**PORTLAND CEMENT CONCRETE  
ALLEY SECTION**

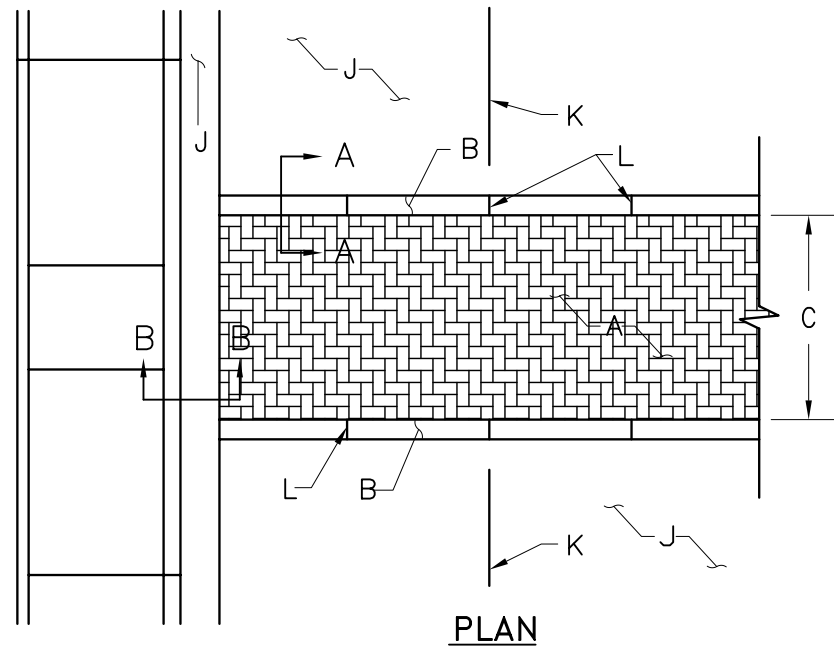
**GENERAL NOTES:**

1. REQUIREMENT FOR COMMERCIAL OR RESIDENTIAL PAVEMENT SECTION SHALL BE DETERMINED BY THE ENGINEER.
2. TRANSVERSE SLOPE OF ALLEY PAVEMENT SURFACE SHALL BE 2% MIN.
3. TYPE AND LOCATION OF JOINTS SHALL BE DEFINED ON THE PROJECT CONSTRUCTION PLANS, SEE SECTION 337.

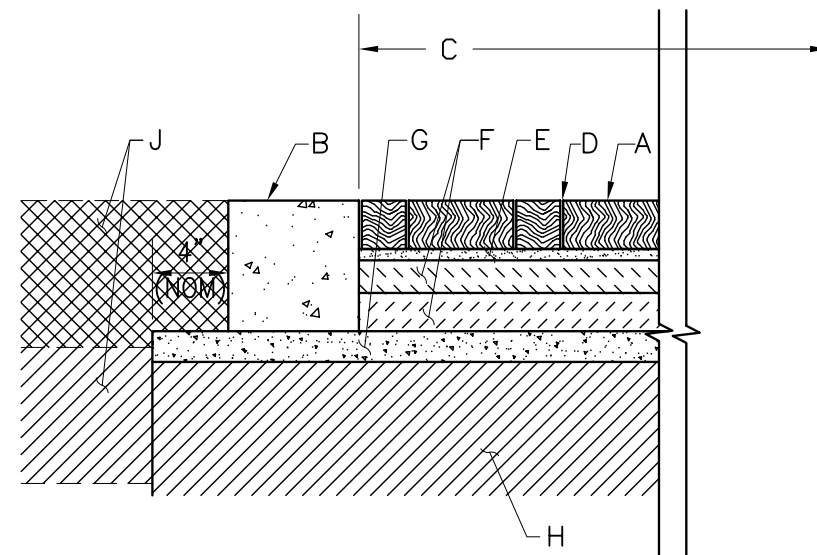
**CONSTRUCTION NOTES:**

- A. ALLEY GUTTER, SEE DWG. 2415.
- B. WALL OR BUILDING FOUNDATION AT PROPERTY LINE.
- C. USE 6" x 18" PORTLAND CEMENT CONCRETE (PCC) CUT-OFF-WALL.
- D. RIGHT-OF-WAY ADJACENT TO OPEN AREA.
- E. USE RESIDENTIAL SECTION FOR RESIDENTIAL ALLEY USE, SEE DWG. 2405.
- F. USE ARTERIAL SECTION FOR COMMERCIAL ALLEY USE, SEE DWG. 2407.
- G. USE 1/2" EXPANSION JOINT WHERE PCC PAVEMENT ABUTS WALLS, RIGID PAVEMENT, POLES, TRANSFORMERS, ETC.
- H. TYPE 4 TIED JOINT, SEE DWG. 2450.
- I. SAWCUT AND SEAL JOINT, SEE DWG. 2450.

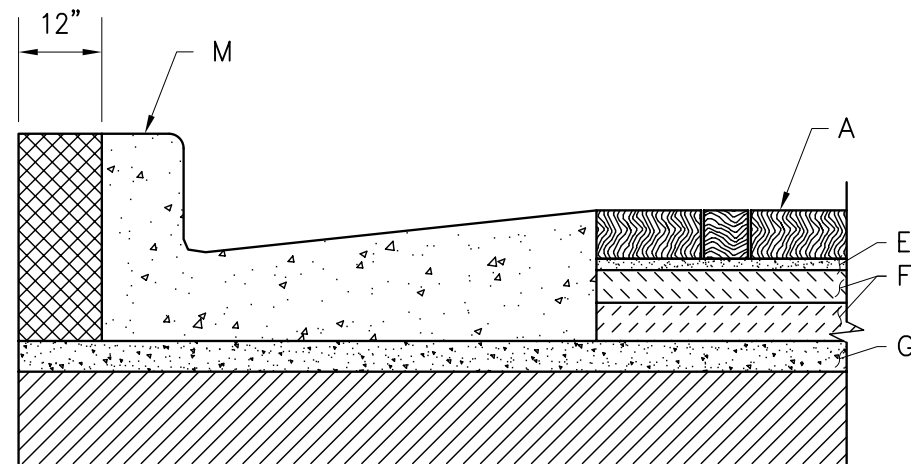
REVISIONS	CITY OF ALBUQUERQUE
1/91 12/15/92	PAVING COMMERCIAL & RESIDENTIAL ALLEY SECTION DWG. 2411 JANUARY 2003



PLAN



SECTION A-A



SECTION B-B

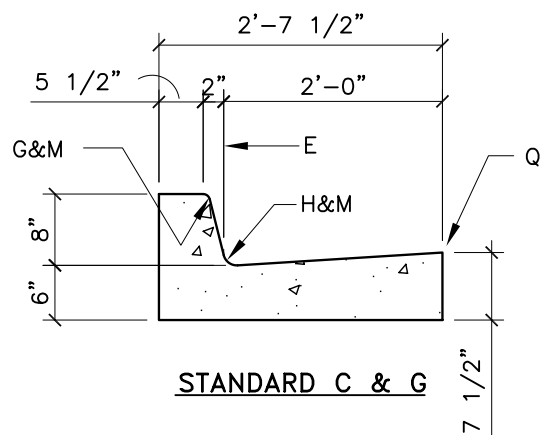
#### CONSTRUCTION NOTES:

- A. 4x8" (NOM) x 3 1/8" CONCRETE BRICK PAVERS,  $f'_m=8000$  psi, COMPLYING WITH REQUIREMENTS OF ASTM C936, STANDARD SPECIFICATIONS FOR SOLID CONCRETE INTERLOCKING PAVER UNITS, COLOR AS SPECIFIED BY THE ENGINEER.
- B. PORTLAND CEMENT CONCRETE EDGE RESTRAINT CURB,  $h=8"$  x  $b=14"$  x  $l=6"$  (NOM) BETWEEN CONTROL JOINTS.
- C. WIDTH OF CROSSWALK BETWEEN RESTRAINT CURBS SHALL BE ADJUSTED SO THAT THE TRIMMING OF CONCRETE BRICK PAVERS WILL NOT BE REQUIRED ADJACENT TO RESTRAINT CURBS.
- D. JOINTS BETWEEN BRICKS SHALL BE APPROX. 1/16" TO 1/8" TO ALLOW FOR SAND FILLER.
- E. BEDDING SAND 1" (NOM) MIN.
- F. 2-2" (NOM) LIFTS, TYPE C OR S-IV ASPHALT CONCRETE (SECTIONS 116, 336)
- G. 1-2" (NOM) LIFT, TYPE B OR S-III ASPHALT CONCRETE (SECTIONS 116, 336)
- H. 12" COMPACTED SUBGRADE, 95% COMPACTION.
- I. STREET PAVEMENT SECTION.
- J. TRAFFIC LANE LINE (TYP).
- K. CONTROL JOINT.
- L. CURB & GUTTER.
- M. GAPS OCCURRING AT THE INTERFACE BETWEEN THE CONCRETE BRICK PAVERS AND ADJACENT CURB & GUTTER AND OTHER MATERIALS SHALL BE FILLED WITH SAW CUT PAVERS WITH A MIN. DIMENSION OF THE PAVER NOT LESS THAN 2". GAPS LESS THAN 3/8" SHALL BE FILLED WITH SAND.

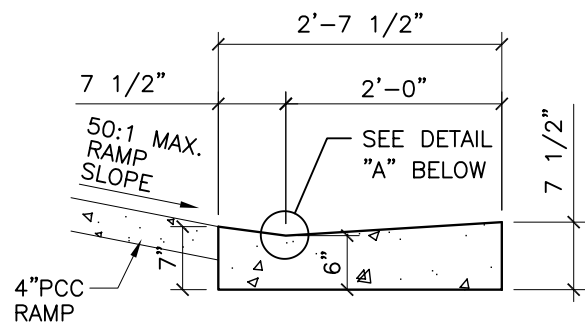
#### GENERAL NOTES

1. CONCRETE PAVERS SHALL BE INSTALLED IN A MODULAR 90° HERRINGBONE PATTERN.
2. EDGE RESTRAINT CURB SHALL HAVE CONTROL JOINTS INSTALLED AT LANE LINES AND THE VENTER OF EACH TRAFFIC LANE CROSSED. IF TRAFFIC LANES ARE NOT DEFINED OF A NON-STANDARD WIDTH CONTROL JOINTS SHALL BE EVENLY SPACED THE LENGTH OF THE RESTRAINING CURB AT 6' (NOM) INTERVALS.
3. BEDDING AND JOINT SAND SHALL BE DRY, WASHED CONCRETE SAND COMPLYING WITH REQUIREMENTS OF ASTM C33, STANDARD SPECIFICATIONS FOR CONCRETE AGGREGATE.
4. WIDTH OF CROSSWALK SHALL BE ADJUSTED SO THAT NO TRIMMING OF CONCRETE PAVERS IS REQUIRED BETWEEN RESTRAINT CURBS.
5. OTHER TYPES OF ACCEPTABLE CONTAINMENT WALLS MAY BE USED WHEN DETAILED ON THE CONSTRUCTION PLANS AND APPROVED BY THE ENGINEER.
6. INSTALLATION PROCESS:
  1. PLACE DRY CONCRETE SAND ON COMPACTED ASPHALT CONCRETE AND SCREED TO A UNIFORM DEPTH NOT LESS THAN 1".
  2. PLACE BRICK PAVERS ON THE CONCRETE SAND IN PATTERN AND JOINT WIDTH(S) SPECIFIED.
  3. VIBRATE PAVERS INTO THE SAND BEDDING WITH A PLATE VIBRATOR. A MINIMUM OF TWO PASSES OF THE VIBRATOR SHALL BE MADE ACROSS THE BRICK SURFACE. VIBRATOR SHALL BE CAPABLE OF 3,000 TO 5,000 LBS. CENTRIFUGAL COMPACTION FORCE, OPERATED AT A FREQUENCY OF 80 TO 90 HERTZ.
  4. SWEEP FILL DRY CONCRETE SAND INTO THE JOINTS AND VIBRATE ACROSS THE BRICK PAVER SURFACE. REPEAT SAND SWEEP FILL UNTIL ALL JOINTS WILL NO LONGER TAKE SAND UNDER THE VIBRATOR ACTION.
  5. VIBRATION SHALL NOT OCCUR WITHIN 3 FEET OF AN UNRESTRAINED EDGE OR LAYING FACES OF THE BRICK SURFACES. ALL BRICK PAVERS PLACED 3 FEET OR GREATER FROM THE LAYING FACE SHALL BE COMPACTED WITH SAND-FILLED JOINTS AT THE COMPLETION OF THE DAY'S WORK. COVER THE REMAINING UNCOMPACTED AREA EXPOSED SAND BEDDING WITH WATERPROOF COVERING.
  6. SWEEP OFF EXCESS SAND WHEN COMPACTION IS COMPLETED.
  7. FINISH SURFACE CONSTRUCTION SHALL NOT DEVIATE FROM THE SPECIFIED ELEVATION BY MORE THAN 3/8" UNDER A 10-FT STRAIGHTEDGE. THE FINISHED ELEVATION OF PAVERS SHALL BE 1/8" TO 1/4" ABOVE ADJACENT DRAINAGE INLETS, EDGE RESTRAINTS, PAVEMENT, AND TOE OF GUTTER PANS, EXCEPT WHERE ADJACENT TO AN ACCESS RAMP WHERE THE PAVER SHALL BE FLUSH TO 1/8" ABOVE THE TOE OF CURB.

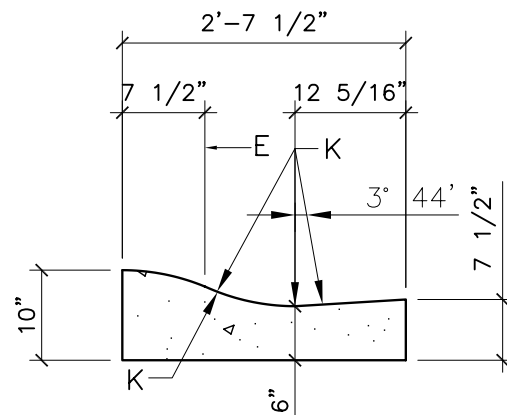
REVISIONS	CITY OF ALBUQUERQUE
	PAVING
	STREET SECTION USING CONCRETE PAVERS
	DWG. 2412 JANUARY 2003



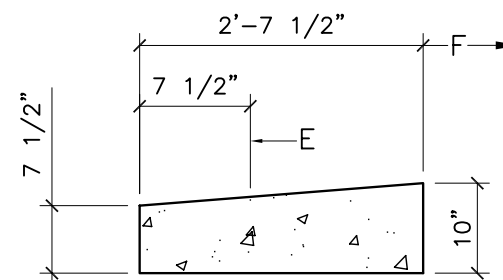
**STANDARD C & G**



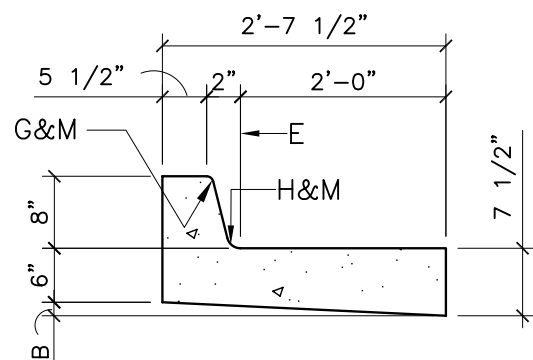
**GUTTER AT (WHEEL CHAIR)  
CURB ACCESS RAMP**



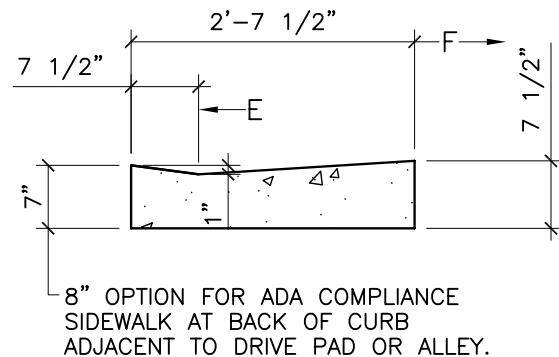
**MOUNTABLE CURB  
ROLL TYPE**



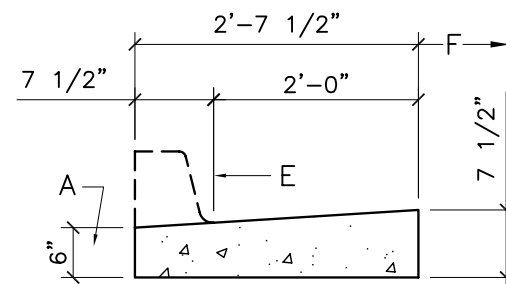
**MOUNTABLE CURB  
ESTATE TYPE**



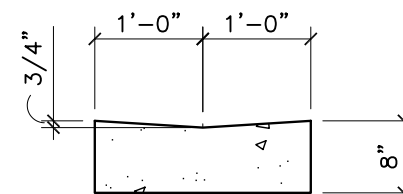
**DEPRESSED GUTTER**



**GUTTER AT DRIVE PAD**

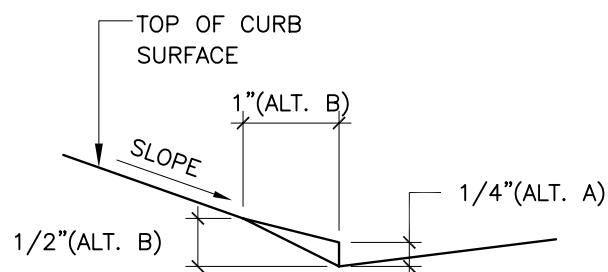


**RUNDOWN CURB**

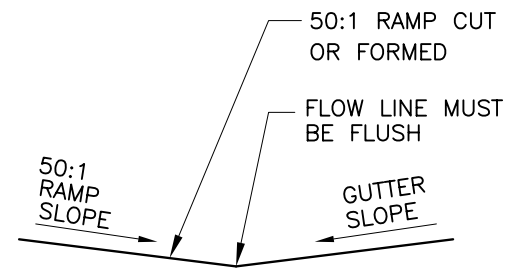


**ALLEY GUTTER**

NOTE:  
ALT. A-1/4" VERT. RISE  
THEN SLOPE UP TO BACK OF  
CURB. (SEE STD. DWG. 2440)  
ALT. B-1/2" RISE 1" HORIZ  
THEN SLOPE UP TO BACK OF  
CURB. (SEE STD. DWG. 2440).



**CURB CUT DETAIL - NOT ADA COMPLIANT**



**CURB CUT DETAIL - ADA COMPLIANT  
DETAIL "A"**

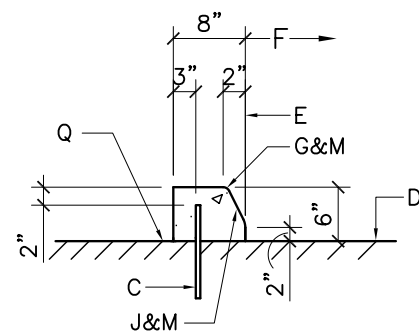
**GENERAL NOTES:**

1. CURB, GUTTER AND CUT-OFF WALL WILL BE CONSTRUCTED OF PORTLAND CEMENT CONCRETE (PCC).
2. FOR STANDARD AND MEDIAN C&G ADJACENT TO ASPHALT CONCRETE (AC) PAVEMENT, PROVIDE CONTRACTION JOINTS AT 12' MAX. SPACING, CONTRACTION JOINTS SHALL BE EITHER SAWED OR TOOLED A MINIMUM OF 1" DEEP AT FINISHED FACES. 1/2" EXPANSION JOINTS TO BE INSTALLED AT CURB RETURNS AND AT A MAXIMUM SPACING OF 200' BETWEEN CURB RETURNS AND SEPARATELY CONSTRUCTED DRIVEWAYS.
3. FOR ALL OTHER C & G CUT-OFF WALL PROVIDE CONTRACTION JOINTS AT 10' MAX SPACING, 1/2" EXP. JTS. AT CURB RETURNS & AT A MAXIMUM SPACING OF 100' BETWEEN CURB RETURNS & EACH SIDE OF SEPARATELY CONSTRUCTED DRIVEWAYS. CONTRACTION JOINTS SHALL BE EITHER SAWED OR TOOLED A MINIMUM OF 1" DEEP AT ALL FINISHED FACES. REINFORCEMENT SHALL NOT BE USED IN CUT-OFF WALLS.
4. FOR C & G CONSTRUCTED WITH PCC PAVEMENT, CONTRACTION JOINTS AND EXPANSION JOINTS SHALL BE THE SAME AS THE PAVEMENT JOINTS.
5. ALL EDGES SHALL BE EDGED WITH A 3/8" RADIUS EDGING TOOL.
6. STANDARD C & G SHALL BE USED FOR NEW CONSTRUCTION UNLESS OTHERWISE AUTHORIZED BY THE CITY ENGINEER.
7. REMOVE & REPLACE PAVEMENT 1" WIDE ADJACENT TO LIP OF GUTTER WHEN CONSTRUCTING C & G ADJACENT TO EXISTING AC PAVEMENT.
8. 1/4" ISOLATION JOINT SHALL BE PLACED BETWEEN SIDEWALK AND C & G WHEN CAST ADJACENT TO EACH OTHER.
9. ADA = AMERICANS WITH DISABILITY ACT.

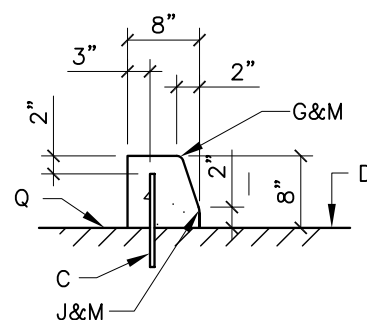
**CONSTRUCTION NOTES:**

SEE COA DRAWING 2415B

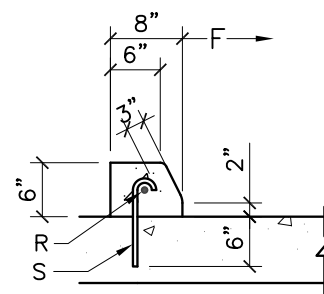
REVISIONS	CITY OF ALBUQUERQUE
9/91	PAVING
11/14/91	CURB AND GUTTER &
12/15/92	CURB CUT DETAILS
3/30/94	DWG. 2415A JANUARY 2003



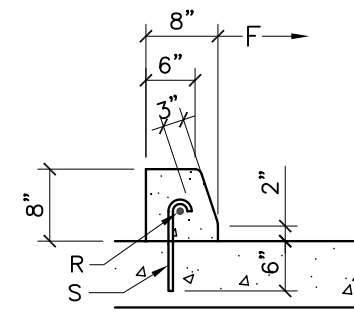
**TYPE I  
PINNED CURB**



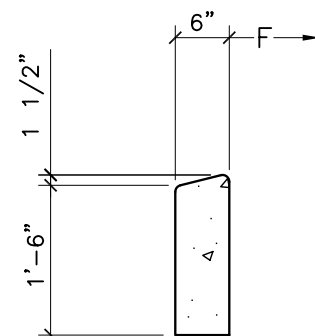
**TYPE II  
PINNED CURB**



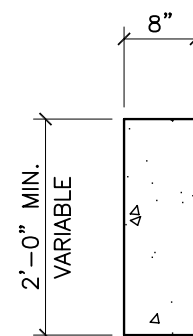
**PINNED CURB TYPE III  
MEDIAN PAVEMENT**



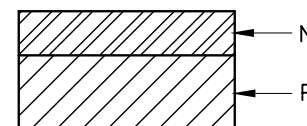
**PINNED CURB TYPE IV  
OUTSIDE PAVEMENT**



**HEADER CURB**

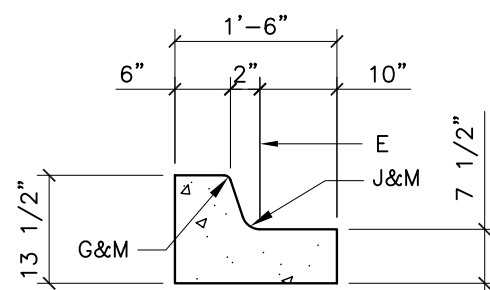


**CUT-OFF WALL**

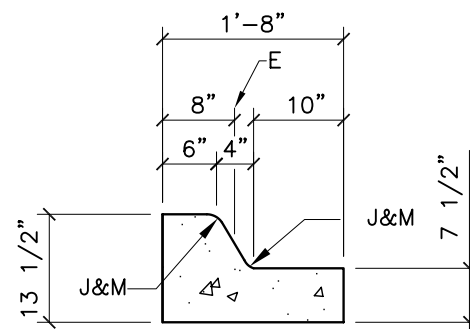


USE ONLY PER DIRECTION  
OR APPROVAL OF THE  
ENGINEER

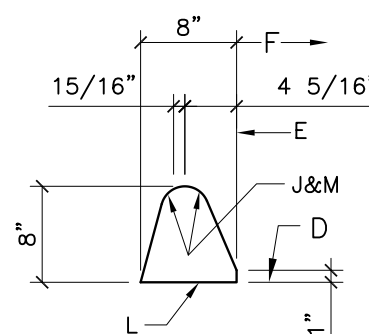
**TEMPORARY OR BICYCLE  
PATH SECTION**



**MEDIAN C & G**



**MOUNTABLE  
MEDIAN CURB**



**TEMPORARY ASPHALT CURB,  
TYPE C RESIDENTIAL**

# **CONSTRUCTION NOTES**

- A. REQ. CONC. CHANNEL LINING, OR CUT-OFF WALL, PROVIDE 1/4" EXP JOINT BETWEEN BACK OF CURB & CHANNEL LINING AND/OR WALL.
- B. VARIABLE, DEPRESS AS NEEDED.
- C. DRIVE NO. 4 PINS 18" DEEP IN HOLES DRILLED @ 2" O.C. IN EXISTING PAVEMENT, SEAL WITH EPOXY.
- D. EXISTING ASPHALT CONCRETE (AC) OR PORTLAND CEMENT CONCRETE (PCC) PAVEMENT.
- E. THEORETICAL FACE OF CURB OR FLOWLINE.
- F. TRAFFIC SIDE.
- G. 3/4" RADIUS.
- H. 1-1/2" RADIUS.
- J. 2" RADIUS.
- K. 24" RADIUS.
- L. TACK COAT.
- M. DIMENSIONS AT ROUNDED CORNERS MEASURED TO INTERSECTION OF STRAIGHT LINES.
- N. 4" AC: MAJOR LOCAL OR BETTER (SP-III)  
3" AC: LOCAL RESIDENTIAL STREET (TYPE C)  
2" AC: BICYCLE PATH (TYPE B, RESIDENTIAL)
- P. 8" SCARIFIED AND COMPACTED SUBGRADE. 95% MINIMUM COMPACTION PER SECTION 301.
- Q. AC PAVEMENT.
- R. #4 CONT. BETWEEN JOINTS 3" COVER AT JOINTS.
- S. #3 PINS @ 3'-0" O.C. W/STD. HOOK.

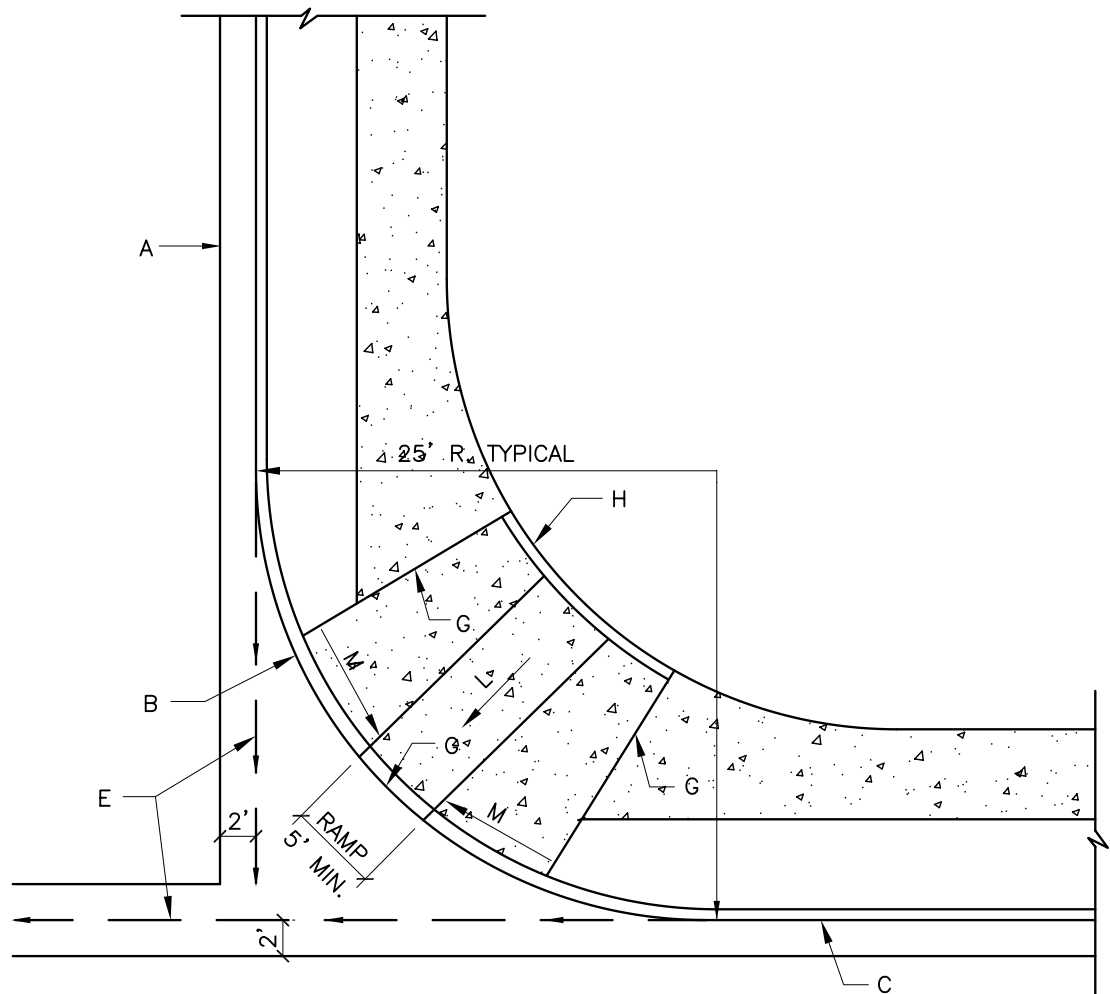
# **GENERAL NOTES**

SEE COA DRAWING 2415A

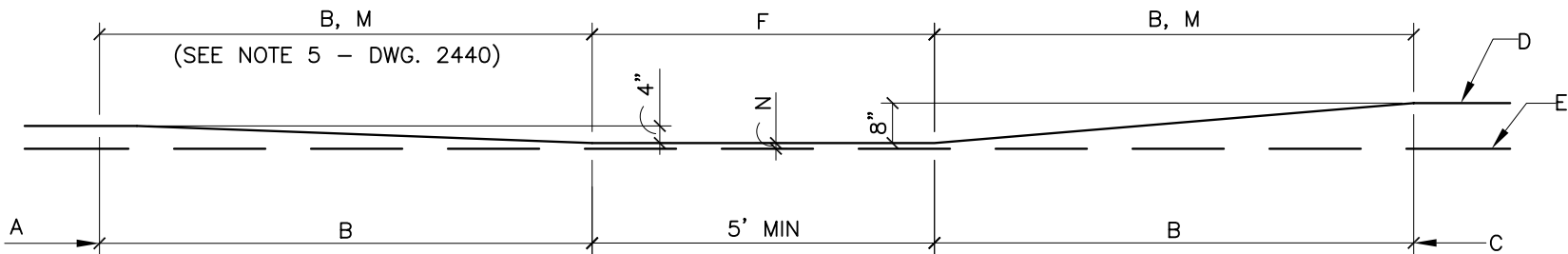
REVISIONS	CITY OF ALBUQUERQUE
9/91 11/14/91 12/15/92 3/30/94	PAVING CURB AND GUTTER & TEMPORARY PAVING SECTION DWG. 2415B JANUARY 2003

**CONSTRUCTION NOTES:**

- A. MOUNTABLE CURB, ROLL TYPE.
- B. CURB TRANSITION. TRANSITION LENGTHS BETWEEN DIFFERENT CURB TYPES SHALL BE 10' MIN.
- C. STANDARD CURB & GUTTER.
- D. TOP OF CURB PROFILE (AT BACK OF CURB).
- E. FLOWLINE.
- F. (WHEELCHAIR RAMP) – CURB ACCESS.
- G. 1/2" EXPANSION JOINT.
- H. HEADER CURB, SEE STD. DWG, 2441 & 2415.
- J. HEADER CURB MAY BE INTEGRAL CURB WITH RAMP. (SEE ALTERNATE SECTION A-A ON STD. DWG. 2441.
- K. FOR CURB ACCESS (WHEELCHAIR) RAMPS AT LOCATIONS NOT INVOLVING CURB TRANSITIONS, SEE STD. DWGS. 2440 & 2441.
- L. 50:1 MAX SLOPE ALL DIRECTIONS.
- M. 12:1 MAX SLOPE.
- N. ACCESS RAMP FLUSH WITH FILLET.

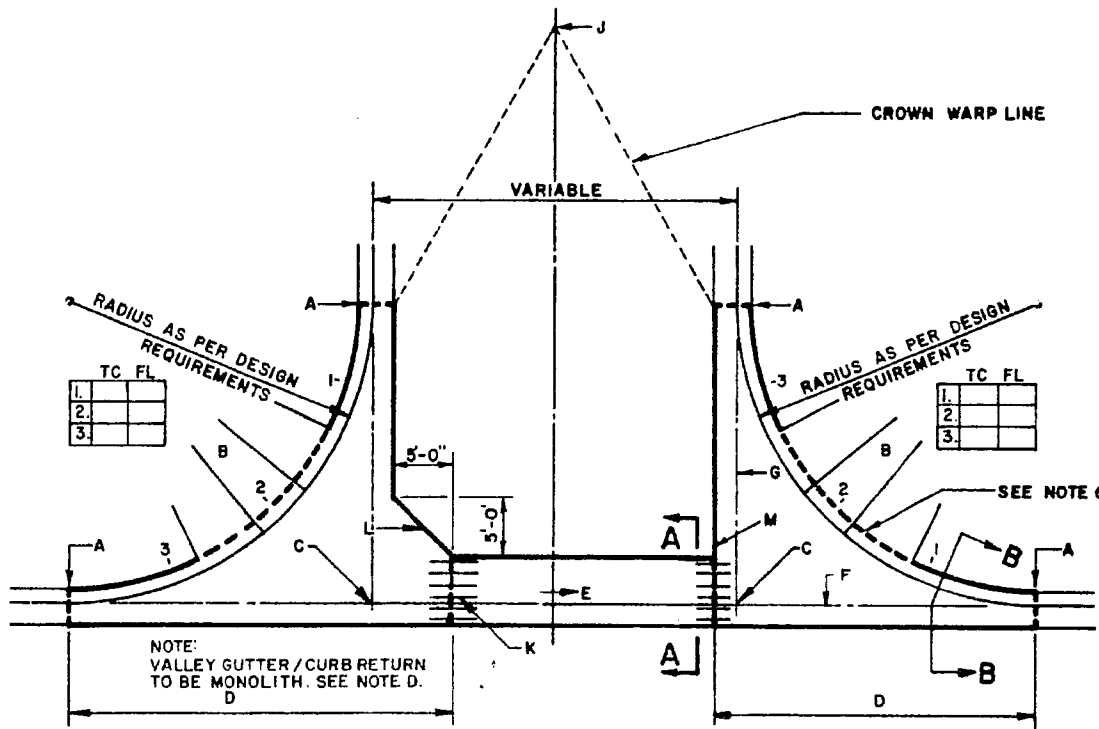


**CURB TRANSITION WITH CURB ACCESS  
(WHEELCHAIR) RAMP**

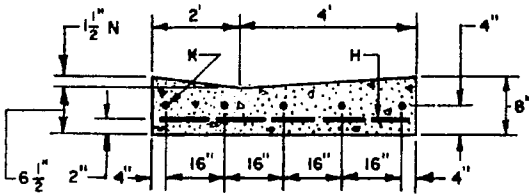


**PROFILE AT BACK OF CURB  
CURB TRANSITION WITH CURB ACCESS RAMP PER DETAIL**

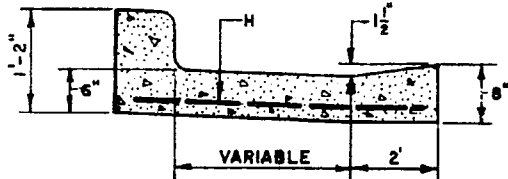
REVISIONS	CITY OF ALBUQUERQUE
1/91 11/14/91 3/30/94	PAVING MOUNTABLE TO STANDARD CURB TRANSITION DWG. 2418      JANUARY 2003



PLAN



SECTION A-A



SECTION B-B

**GENERAL NOTES**

1. DESIGN ELEVATIONS TO BE GIVEN AT EACH END OF THE CURB RETURN (TOP OF CURB ELEV.) AND AT INTERSECTIONS OF PROJECTED FLOWLINES (FLOWLINE ELEV.).
2. ON UPSTREAM AND DOWNSTREAM ENDS OF THE INTERSECTION, VALLEY GUTTER CONSTRUCTION SHALL EXTEND TO THE END OF RETURNS.
3. THE VALLEY GUTTER TO BE REINFORCED WITH 6" X 6" X NO. 6 GA. WIRE MESH.
4. INVERT OF VALLEY GUTTER TO EXTEND FROM FLOWLINE OF UPSTREAM CURB RETURN TO FLOWLINE OF DOWNSTREAM CURB RETURN.
5. CURB FLOWLINE AND TOP OF CURB ELEV. SHOWN IN THE BOX CORRESPOND TO QUARTERPOINTS INDICATED ON THE CURB RETURN IN THE CLOCKWISE DIRECTION.
6. --- DENOTES 1/2" EXPANSION JOINT.
7. FOR NEW CONSTRUCTION, VALLEY GUTTER SHALL BE CONSTRUCTED PRIOR TO ADJACENT PAVEMENT. ASPHALT CONCRETE SHALL BE INSTALLED MONOLITHICALLY TO MEET NEW VALLEY GUTTER.
8. PRIOR TO CONSTRUCTION OF NEW VALLEY GUTTER ON EXISTING ACCEPTED STREETS, PAVEMENT SHALL BE REMOVED AS SHOWN ON PLANS.

**CONSTRUCTION NOTES**

- A. END OF CURB RETURN, SEE NOTE 1.
- B. FOR RAMP DETAILS, SEE DWGS. 2418, 2440, 2441.
- C. INTERSECTION OF FLOWLINES, SEE NOTE 1.
- D. VALLEY GUTTER (CURB RETURN/FILLET)
- E. DIRECTION OF FLOW.
- F. FLOWLINE.
- G. PROJECTED FLOWLINE OF 1-1/2" INVERT, SEE NOTE 2.
- H. 6" X 6" X NO. 6 GA. WIRE MESH.
- J. BEGIN CROWN WARP TO NO CROWN SECTION AS PER DWGS. 2401 OR AS SPECIFIED ON PLANS, OR INDICATED BY THE ENGINEER.
- K. NO. 4 BARS 3'-0" LONG AT 16" O.C.
- L. ALTERNATE A, WITH FILLET AS PER PLANS.
- M. ALTERNATE B, NO FILLET AS PER PLANS.
- N. THE 1-1/2" INVERT DEPTH MAY BE REDUCED TO IMPROVE RIDEABILITY WITH APPROVAL OF ENGINEER.

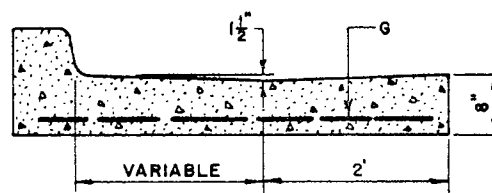
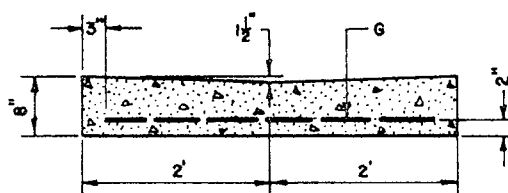
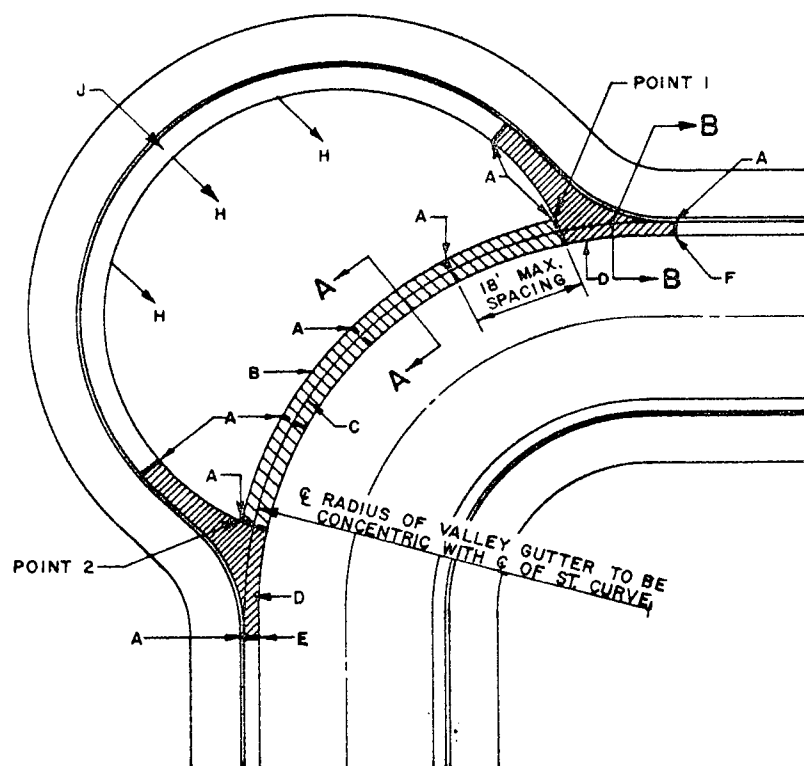
REVISIONS 1/91 11/14/91 3/30/94		CITY OF ALBUQUERQUE <b>PAVING</b> <b>CONCRETE VALLEY GUTTER</b> <b>DWG. 2420</b> AUG. 1986
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# GENERAL NOTES

1. FLOWLINE AND T.C. ELEV. TO BE GIVEN AT QUARTERPOINTS FROM CURB RETURN "A" TO CURB RETURN "B" IN THE CLOCKWISE DIRECTION.
2. INV. OF VALLEY GUTTER TO EXTEND FROM FLOWLINE OF UPSTREAM CURB RETURN TO FLOWLINE OF DOWNSTREAM CURB RETURN.
3. ENTIRE VALLEY GUTTER TO BE REINFORCED WITH 6" X 6" X NO. 6 GA. WIRE MESH.
4. - - - DENOTES 1/2" PREWOLDED BIT. EXPANSION JOINT.

# CONSTRUCTION NOTES

- A. EXPANSION JOINT (MAX. 18 FT., O.C.).
- B. VALLEY GUTTER.
- C. FLOWLINE.
- D. MONOLITHIC CONSTRUCTION (INCLUDING CURB).
- E. CURB RETURN "A".
- F. CURB RETURN "B".
- G. 6" X 6" X NO. 6 GA. WIRE MESH.
- H. SLOPE PAVING TO VALLEY GUTTER.
- J. GUTTER WILL BE DEPRESSED FROM POINT 1 TO POINT 2.



REVISIONS	
1/91	
11/14/91	

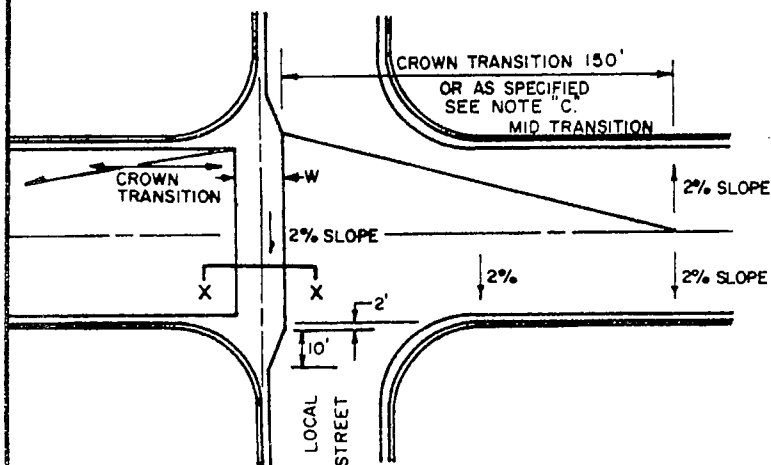
CITY OF ALBUQUERQUE

PAVING

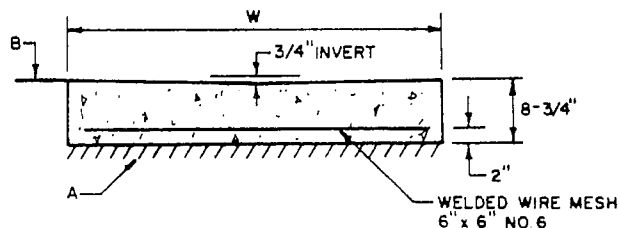
CONCRETE VALLEY GUTTER

DWG. 2421

AUG. 1986



HIGHER SPEED ROADWAY



SECTION X-X

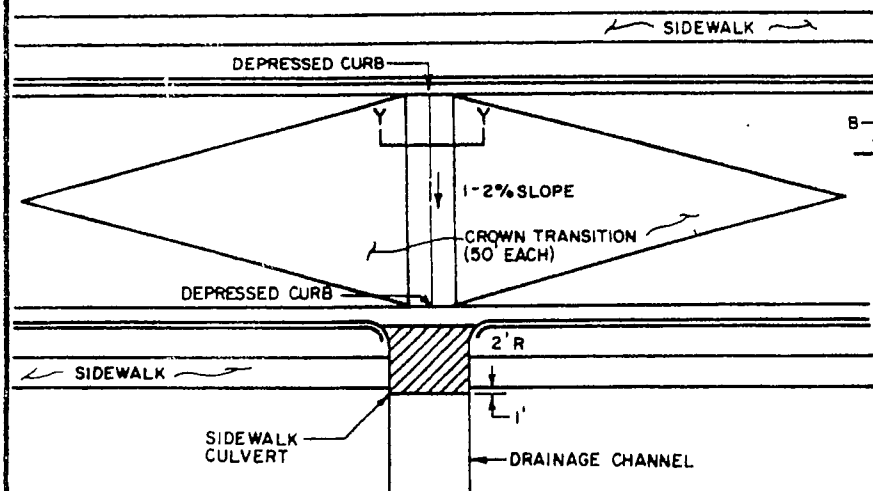
DESIGN SPEED	'W' GUTTER WIDTH	CROWN TRANSITION RATE
35 MPH	12'	1:150
50 MPH	16'	1:200

GENERAL NOTES

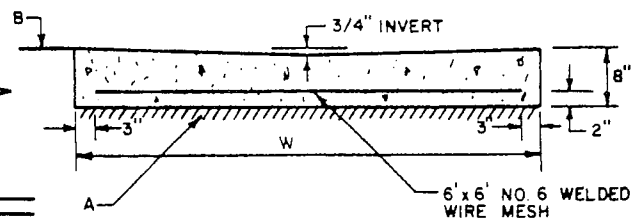
1. VALLEY GUTTER SHOWN IN THIS DRAWING ARE TO BE USED WHERE THERE IS A NON STOPPING CONDITION FOR VEHICLES CROSSING THE VALLEY GUTTER.
2. VALLEY GUTTERS ARE NO TO BE USED AS STANDARD DESIGN FOR CROSSING WATER ACROSS COLLECTOR OR ARTERIAL ROADWAYS EXCEPT WITH WRITTEN AUTHORIZATION FROM THE CITY TRAFFIC ENGINEER.
3. REFER TO OTHER CITY OF ALBUQUERQUE STANDARD DRAWINGS FOR CURB & GUTTER AND PAVING CONSTRUCTION DETAILS.
4. SPECIAL VALLEY GUTTERS SHALL BE P.C.C. (SEE SECTION 101).

CONSTRUCTION NOTES

- A. FOUNDATION FOR SPECIAL VALLEY GUTTERS SHALL BE EQUAL TO BASE AND SUBGRADE REQUIREMENTS FOR ADJACENT PAVEMENT SECTION BELOW BOTTOM OF GUTTER, EXCEPT IN NO CASE SHALL IT BE LESS THAN 12" OF COMPACTED SUBGRADE (SEE SECTION 301).
- B. SPECIAL VALLEY GUTTERS SHALL BE COMPLETED PRIOR TO PLACEMENT OF ADJACENT ASPHALT SURFACE COURSE.
- C. TRANSITION LENGTHS TO BE CALCULATED PER TABLE.



LOCAL STREET (25 MPH DESIGN SPEED)



SECTION Y-Y

REVISIONS  
1/91  
11/14/91

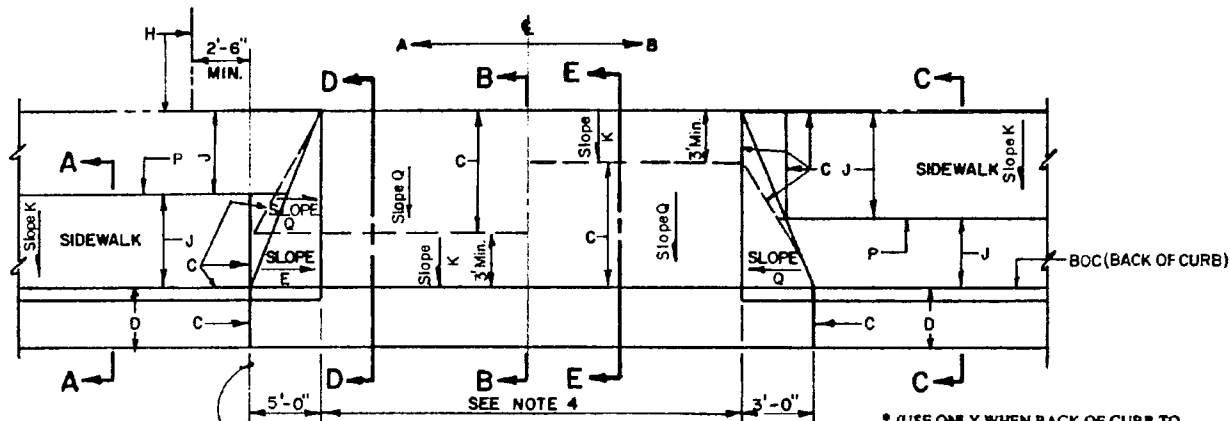
CITY OF ALBUQUERQUE

PAVING  
SPECIAL VALLEY GUTTERS

DWG. 2422

AUG. 1986

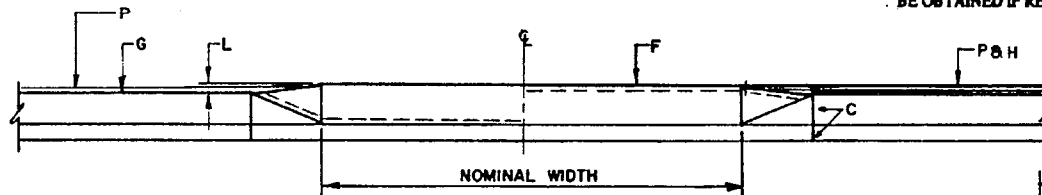




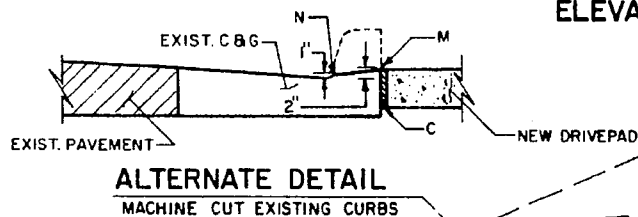
PLAN

ADA WITH GUTTER OPTION  
AT DRIVEPADS (OTHER-  
WISE 7'-0") SEE STD.  
DETAIL DWG 2415.

(USE ONLY WHEN BACK OF CURB TO  
PROPERTY LINE IS 9' OR MORE.  
DRIVEPADS WITH LESS THAN 9'-0"  
IN DEPTH REQUIRE A SPECIAL DESIGN  
TO BE SUBMITTED TO AND APPROVED  
BY THE CITY ENGINEER. ADDITIONAL  
RIGHT-OF-WAY OR EASEMENTS SHALL  
BE OBTAINED IF REQUIRED.)

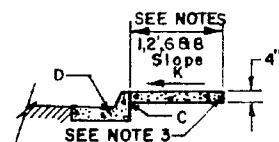


ELEVATION

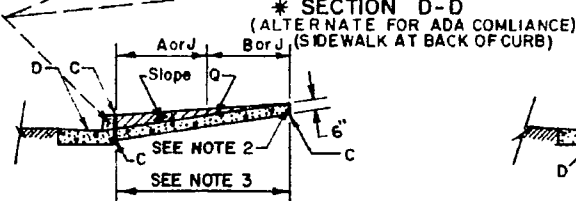


ALTERNATE DETAIL

MACHINE CUT EXISTING CURBS

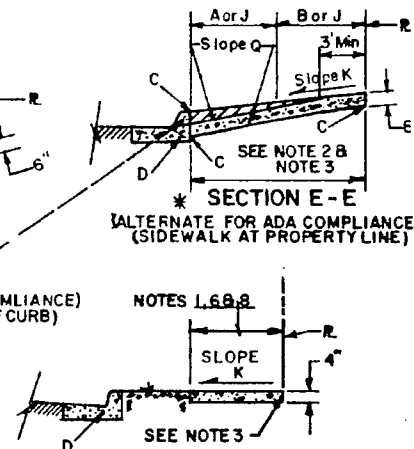


SECTION A-A



SECTION B-B

(ADA COMPLIANCE NOT  
REQUIRED OR POSSIBLE  
WO/ADDITIONAL ROW ACQUIS)



SECTION C-C

# GENERAL NOTES

1. DEVIATIONS FROM THESE STANDARDS SHALL BE SUBMITTED TO THE CITY ENGINEER AND/OR TRAFFIC ENGINEER FOR APPROVAL PRIOR TO CONSTRUCTION.
2. REQUEST FOR SIDEWALK VARIANCES SHALL BE SUBMITTED TO THE DEVELOPMENT REVIEW BOARD.
3. USE 1/2" EXP. JT. WHERE SIDEWALK OR DRIVEPAD ABUTS BLDGS., FENCES, WALLS OR OTHER IMMOVABLE OBJECTS.
4. ALL DRIVEPADS SHALL BE A MIN. THICKNESS OF 6" AND SHALL BE CONSTRUCTED FROM BACK OF CURB TO P/L.
5. DRIVEPADS WIDER THAN 18' (NOMINAL) SHALL HAVE A 1/2" EXP. JT AT MIDPOINT. DRIVEPADS WIDER THAN 36' SHALL HAVE 2 OR MORE 1/2" EXP. JTS. EQUALLY SPACED, MAX., SPACING IS 18' APART.
6. SIDEWALK AT THE BACK OF CURB SHALL BE USED ONLY WHEN VARIANCE IS APPROVED.
7. FOR SIDEWALK WIDTH, SEE CHAPTER 23 THE DEVELOPMENT PROCESS MANUAL.
8. SUBGRADE UNDER SIDEWALK & DRIVEPAD SHALL BE COMPACTED AS PER SECTION 301.
9. ADA - AMERICANS WITH DISABILITIES ACT.

## CONSTRUCTION NOTES

- A. CURB TYPE SIDEWALK.
- B. OFFSET SIDEWALK.
- C. 1/2" EXPANSION JOINT ADJACENT TO FIELD CONDITIONS ON REPLACEMENT WORK.
- D. CURB AND GUTTER.
- E. SLOPE TO BE ADJUSTED TO PROVIDE A UNIFORM TRANSITION BETWEEN SIDEWALK AND DRIVEPAD. (NOT TO EXCEED 1(VERTICAL) TO 10(HORIZONTAL)).
- F. TOP OF DRIVEPAD.
- G. TOP OF CURB.
- H. PROPERTY LINE.
- J. VARIABLE WIDTH.
- K. SLOPE 1(VERTICAL) TO 50(HORIZONTAL).
- L. THE FINISH GRADE ELEVATION DIFFERENCE BETWEEN TOP OF DRIVEPAD AT PROPERTY LINE AND TOP OF CURB AS DETERMINED BY A SLOPE OF 1(VERTICAL) TO 50(HORIZONTAL) FROM TOP OF CURB TO TOP OF DRIVEPAD, AND ANY DEVIATION FROM THIS SLOPE MUST BE APPROVED BY THE CITY ENGINEER.
- M. SAW CUT EXISTING CONCRETE FROM BACKSIDE OF CURB WITH SLOPE TOWARD FLOWLINE.
- N. EXPOSED CUT EDGES SHALL BE GROUND SMOOTH/ ROUNDED TO REMOVE SHARP EDGE.
- P. OUTSIDE EDGE OF SIDEWALK.
- Q. SLOPE REQUIRED TO MEET GIVEN OR SET BOUNDARY ELEVATIONS (PROPERTY LINE OR BOC, ETC.).

CITY OF ALBUQUERQUE

PAVING  
DRIVEPADS

DWG. 2425

AUG. 1986

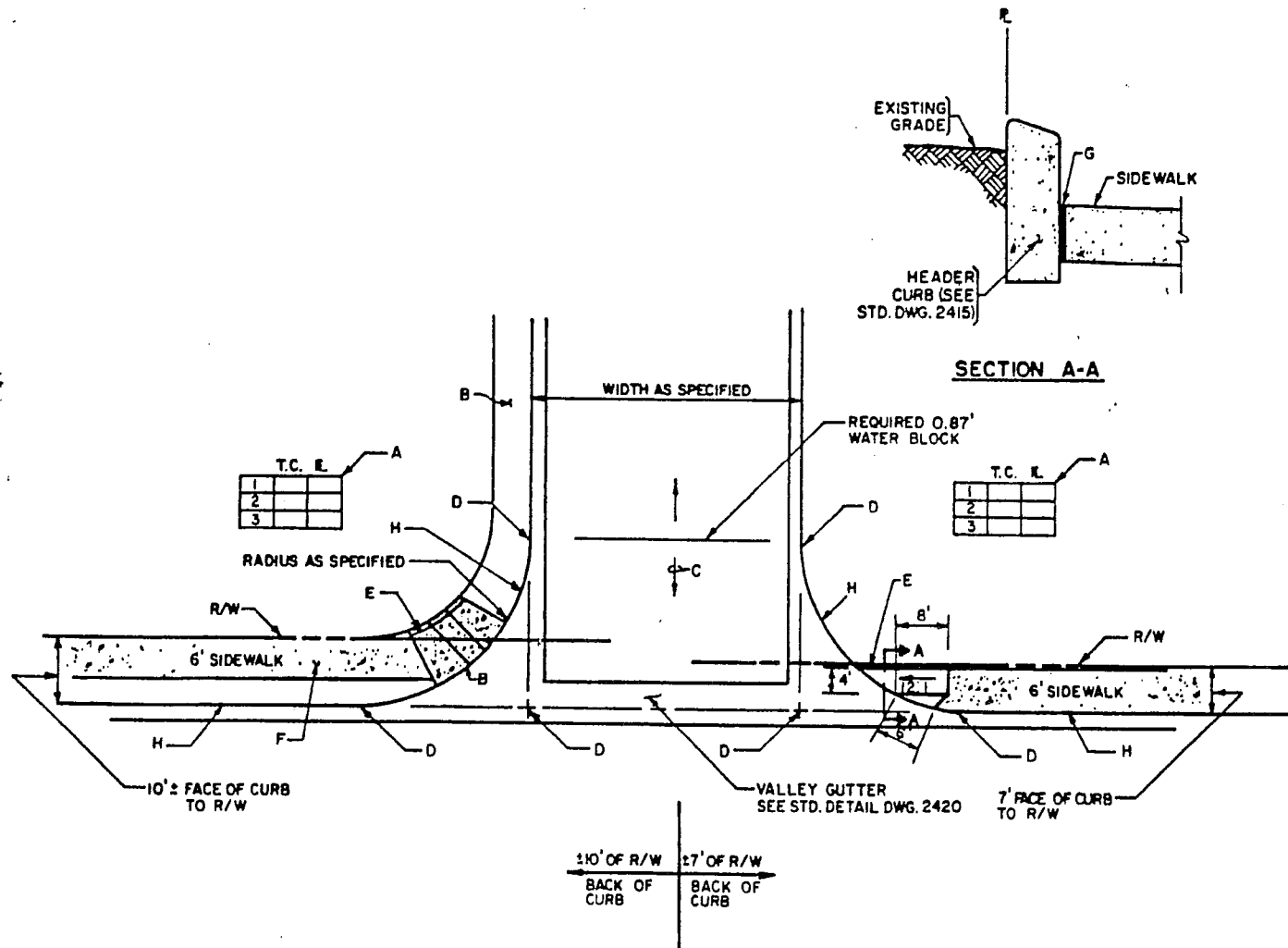
REVISIONS	
1/91	
11/14/91	
3/30/94	

## GENERAL NOTES:

1. THESE DETAILS ARE PROVIDED FOR HIGH TRAFFIC VOLUME PRIVATE ENTRANCES TO COMMERCIAL SITES AND THE LIKE, IN LIEU OF STANDARD DRIVEPADS.

## CONSTRUCTION NOTES:

- A. INCLUDE QUARTER POINT ELEVATIONS. SEE STD. DETAIL DWG. 2420.
- B. WHERE INTERIOR SIDEWALK CONNECTION IS TO BE PROVIDED - CONSTRUCT CURB ACCESS RAMPS AS PER STD. DETAIL DWGS 2418 & 2441.
- C. INITIAL GRADE TO BE 4% OR LESS WHEN CONNECTING TO COLLECTOR OR ARTERIAL STREETS. 6% OR LESS WHEN CONNECTING TO LOCAL STREETS.
- D. INCLUDE ELEVATIONS AT EACH END OF CURB RETURN AND INTERSECTIONS OF PROJECTED FLOWLINES. SEE STD. DWG. 2420.
- E. AT PROPERTY LINE, CONSTRUCT HEADER CURB. SEE STD. DWG. 2415.
- F. IF SIDEWALK IS AGAINST CURB, THE SIDEWALK SHOULD BE TRANSITIONED TO KEEP THE CURB ACCESS RAMP IN THE LOCATION SHOWN.
- G. 1/2" EXPANSION JOINT MATERIAL.
- H. THEORETICAL FACE OF CURB OR FLOWLINE.



PRIVATE ENTRANCE

CITY OF ALBUQUERQUE

REVISIONS

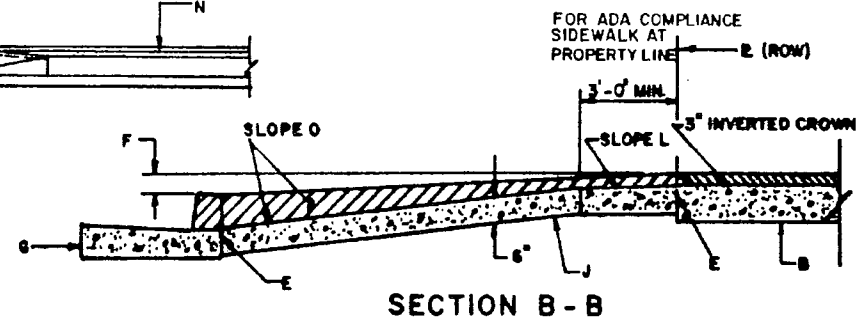
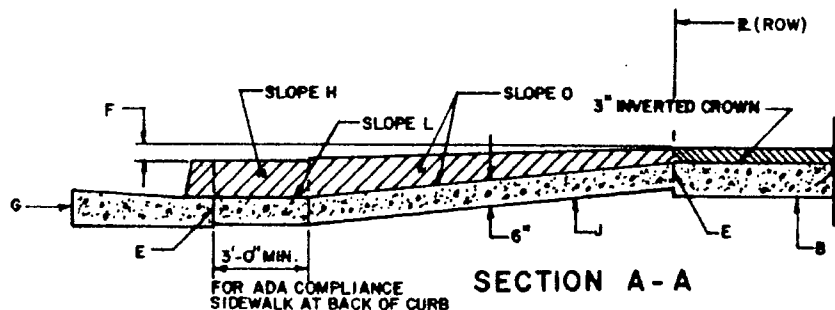
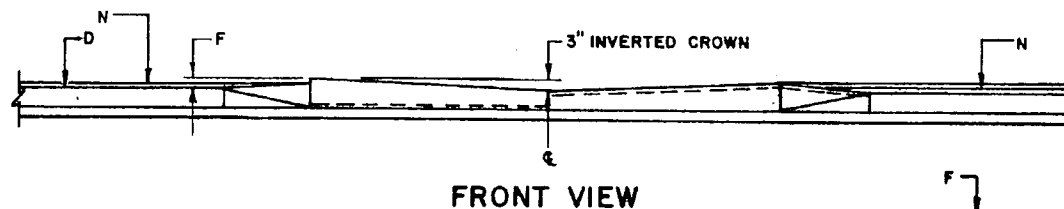
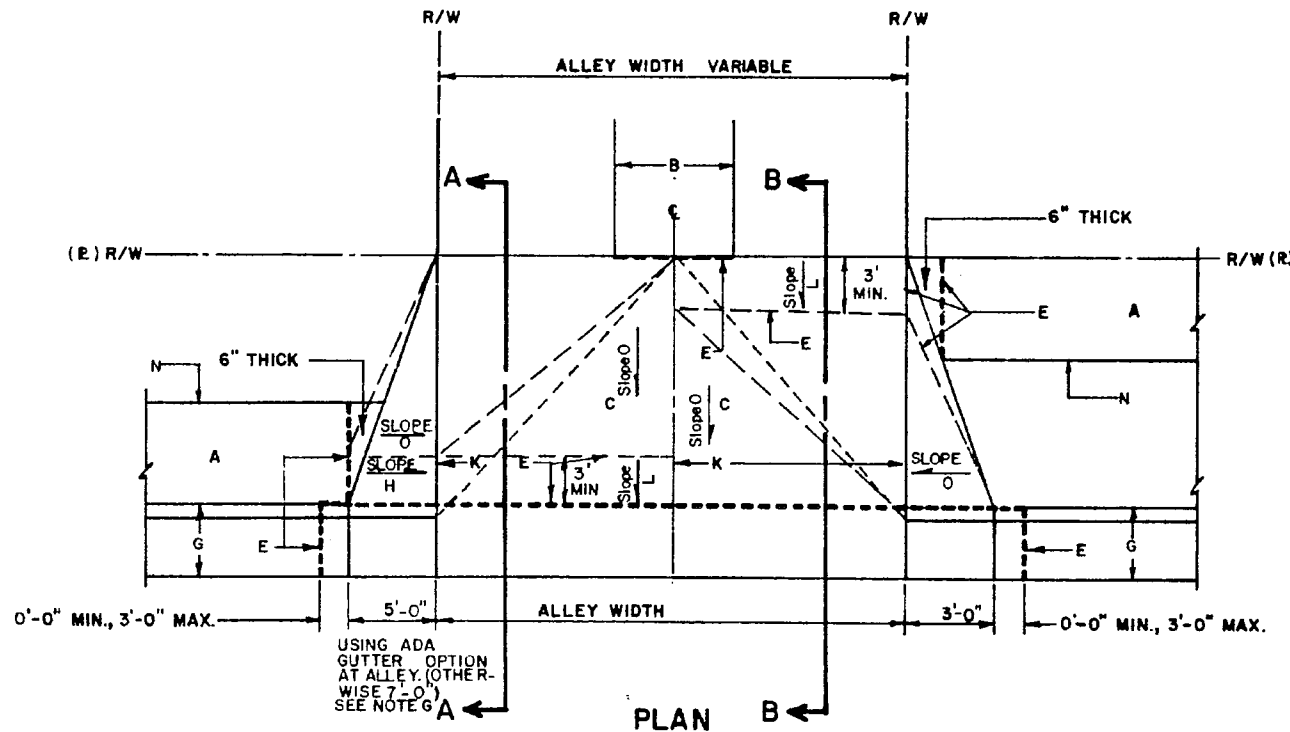
1/91  
11/14/91  
4/4/94

PAVING  
PRIVATE ENTRANCE DETAILS -  
ILLUSTRATING TWO SEPRATE R/W  
CONDITIONS.  
DWG. 2426

AUG. 1986

# CONSTRUCTION NOTES

- A. SIDEWALK
- B. ALLEY GUTTER, SEE DWGS 2411 & 2415
- C. TRANSITION FROM 3" INVERTED ALLEY CROWN TO NO CROWN AT BACK OF CURB.
- D. TOP OF CURB.
- E. 1/2" EXPANSION JOINT.
- F. THE FINISH GRADE ELEVATION DIFFERENCE BETWEEN TOP OF ALLEY AT PROPERTY LINE AND TOP OF CURB AS DETERMINED BY A SLOPE OF 1(VERTICAL) TO 50(HORIZONTAL) FROM TOP OF CURB UP TO TOP OF ALLEY.
- G. CURB AND GUTTER (SEE STD DWG 2415 ADA OPTION)
- H. SLOPE TO BE ADJUSTED TO PROVIDE A UNIFORM TRANSITION BETWEEN DRIVEPAD AND SIDEWALK (NOT TO EXCEED 1(VERTICAL) TO 10(HORIZONTAL) UNITS OF MEASURE FOR ADA COMPLIANCE).
- J. DRIVEPAD, CONSTRUCT WITH PORTLAND CEMENT CONCRETE (SEE SECTION 101).
- K. WEAKENED PLANE, (SAWCUT OR SCORE TO 1/4" DEPTH OF CONCRETE SLAB).
- L. SLOPE NOT TO EXCEED 1(VERTICAL) TO 50(HORIZONTAL) UNITS OF IDENTICAL MEASURE.
- M. ADA = AMERICAN WITH DISABILITIES ACT.
- N. EDGE OF SIDEWALK (FRONT OR BACK).
- O. STRAIGHT LINE SLOPE REQUIRED TO MEET GIVEN OR SET BOUNDARY ELEVATIONS (I.E., PROPERTY LINE, BACK OF CURB, ECT).



SECTION B - B

REVISIONS
1/91
11/14/91
1/94

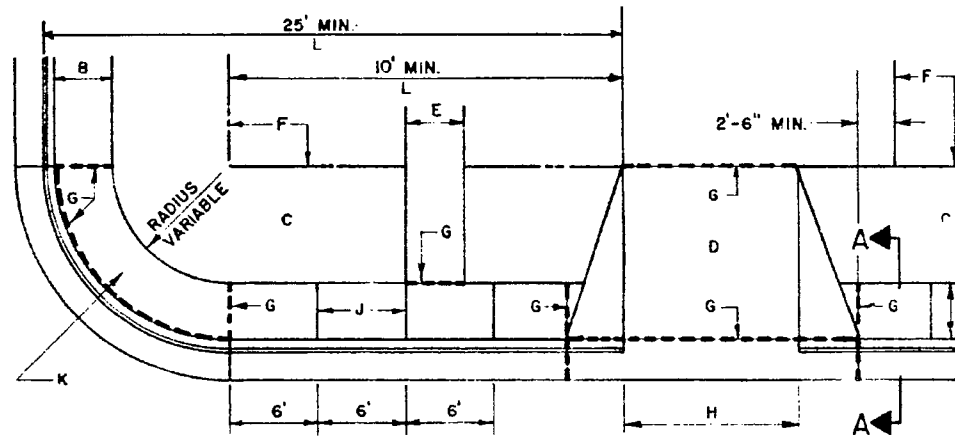
CITY OF ALBUQUERQUE

PAVING

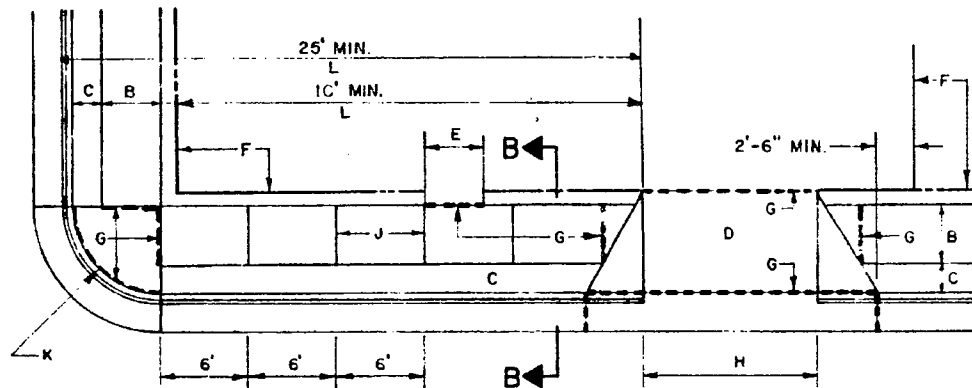
ALLEY INTERSECTION

DWG. 2428

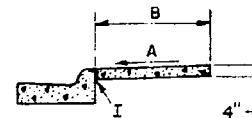
AUG. 1986



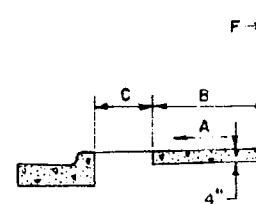
PLAN  
CURB TYPE SIDEWALK



PLAN  
OFFSET TYPE SIDEWALK



SECTION A-A



SECTION B-B

#### GENERAL NOTES

1. DEVIATIONS FROM THESE STANDARDS SHALL BE SUBMITTED TO THE CITY ENGINEER AND/OR CITY TRAFFIC ENGINEER FOR APPROVAL PRIOR TO CONSTRUCTION.
2. SUBGRADE UNDER SIDEWALKS AND DRIVEPADS SHALL BE COMPACTED IN ACCORDANCE WITH SECTION 301.

#### CONSTRUCTION NOTES

- A. SLOPE 1(VERTICAL) TO 50(HORIZONTAL).
- B. SIDEWALK WIDTHS SHALL BE IN ACCORDANCE WITH CHAPTER 23 OF THE DEVELOPMENT PROCESS MANUAL.
- C. SETBACK TO BE DETERMINED BY AVAILABLE RIGHT-OF-WAY. SEE CHAPTER 23 OF DEVELOPMENT PROCESS MANUAL.
- D. SEE DRIVEPAD DETAIL, DWG 2425.
- E. WALKWAY VARIABLE.
- F. PROPERTY LINE.
- G. X" EXPANSION JOINTS WHERE SIDEWALK OR DRIVEPAD ABUTS BUILDINGS, FENCES, WALLS OR OTHER IMMOVABLE OBJECTS.
- H. 12 FT. MIN., 22 FT. MAX. - RESIDENTIAL.  
12 FT. MIN., 25 FT. MAX. - LIGHT COMMERCIAL.  
20 FT. MIN., 35 FT. MAX. - HEAVY COMMERCIAL.
- J. CONTRACTION JOINTS.
- K. FOR CURB ACCESS RAMPS, SEE DWGS 2440 & 2441.
- L. CHECK DIMENSION FROM BOTH PROPERTY LINE AND FLOW LINE. USE IN AREAS WHERE DRIVEPAD IS FARTHEST FROM INTERSECTION.

REVISIONS
1. 11/12/91
2. 4/12/94

CITY OF ALBUQUERQUE

PAVING  
SIDEWALK DETAILS  
DWG. 2430

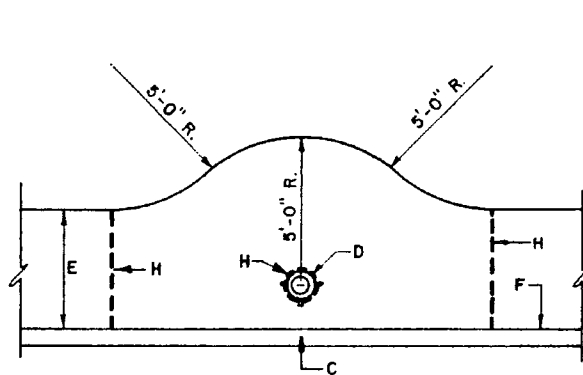
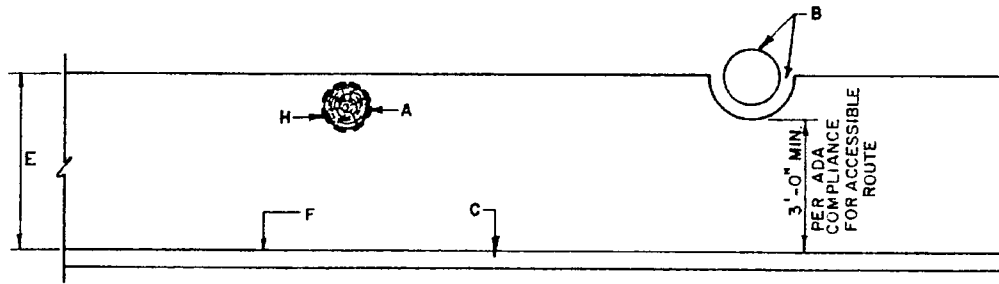
AUG. 1986

## GENERAL NOTES:

1. FOR SIDEWALK CONSTRUCTION DETAILS, SEE DWG. 2430.
2. USE WHERE AVAILABLE R/W EXIST, TO BE DETERMINED BY THE ENGINEER.
3. PROVIDE  $\frac{1}{2}$ " PREFORMED EXPANSION JOINT MATERIAL AROUND ALL POWER POLES AND FIRE HYDRANTS WITHIN THE SIDEWALK AREA.

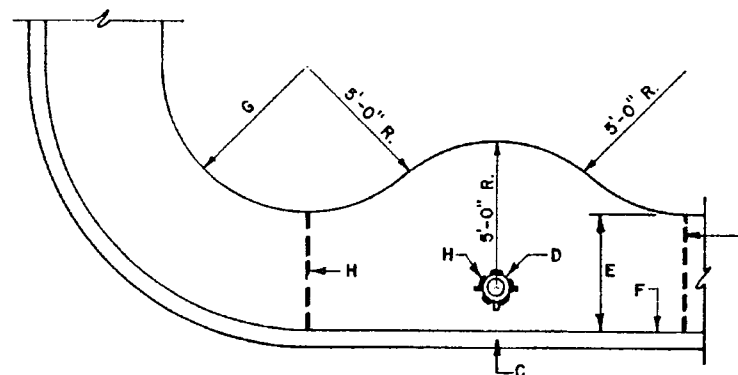
## CONSTRUCTION NOTES:

- A. POWER POLE.
- B. LEAVE 6" CLEARANCE ALL AROUND TREE TRUNK.
- C. TOP OF CURB.
- D. FIRE HYDRANT.
- E. SIDEWALK.
- F. BACK OF CURB.
- G. EXTERIOR EDGE OF SIDEWALK TO BE TANGENT TO ARCS.
- H.  $\frac{1}{2}$ " EXPANSION JOINT MATERIAL.



ON STRAIGHT STRETCH

4'-0" SIDEWALK ENCLOSING A FIRE HYDRANT



AT CURB RETURN

REVISIONS
11/14/91
4/12/94

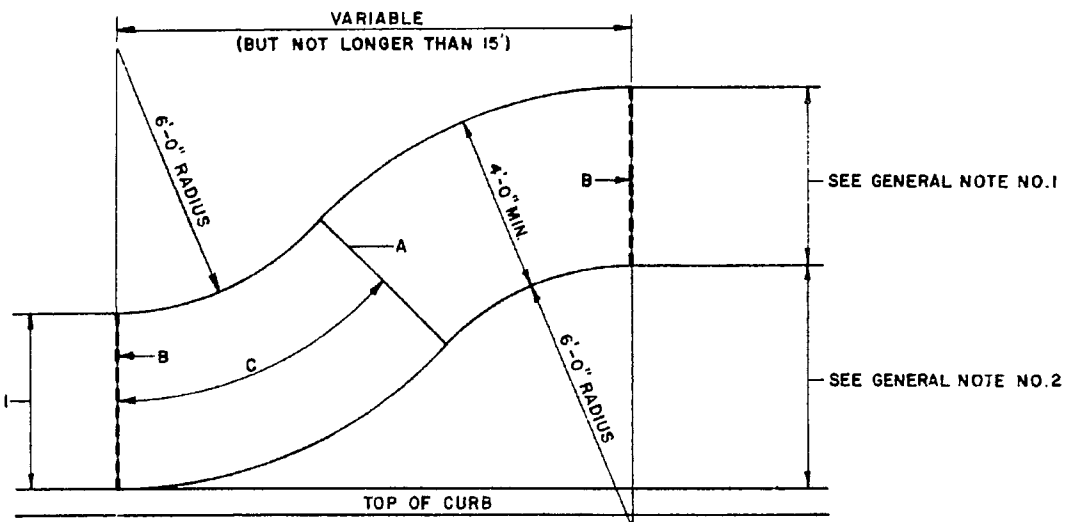
CITY OF ALBUQUERQUE

PAVING

SIDEWALK OBSTRUCTIONS

DWG. 2431

AUG. 1986



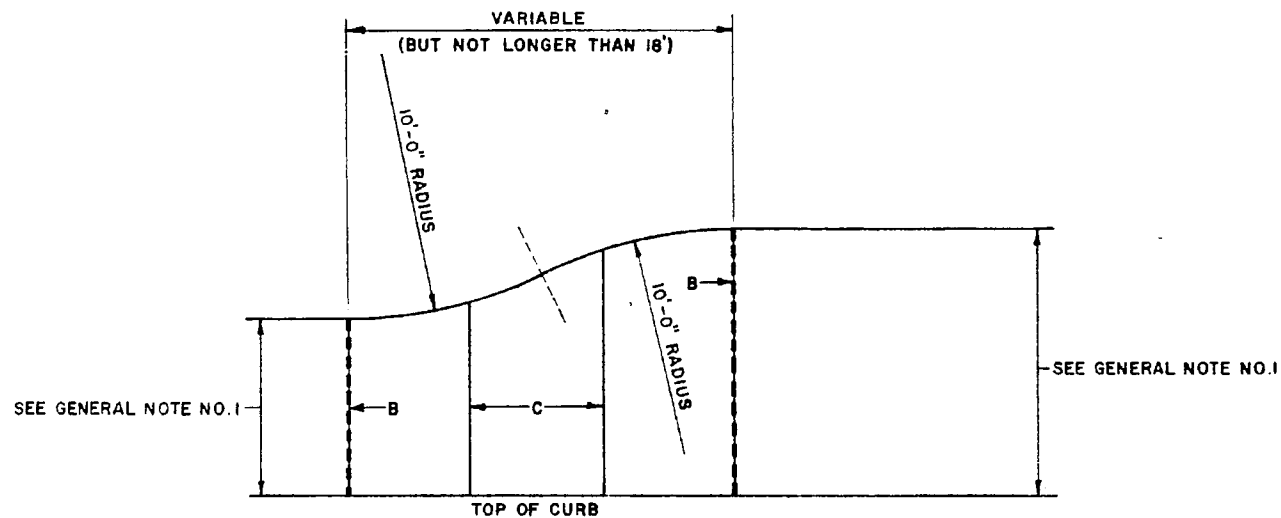
CURB TYPE TO OFFSET TYPE

### GENERAL NOTES:

1. FOR SIDEWALK CONSTRUCTION DETAILS SEE DWG. 2430.
2. SETBACK TO BE DETERMINED BY AVAILABLE R/W.

### CONSTRUCTION NOTES:

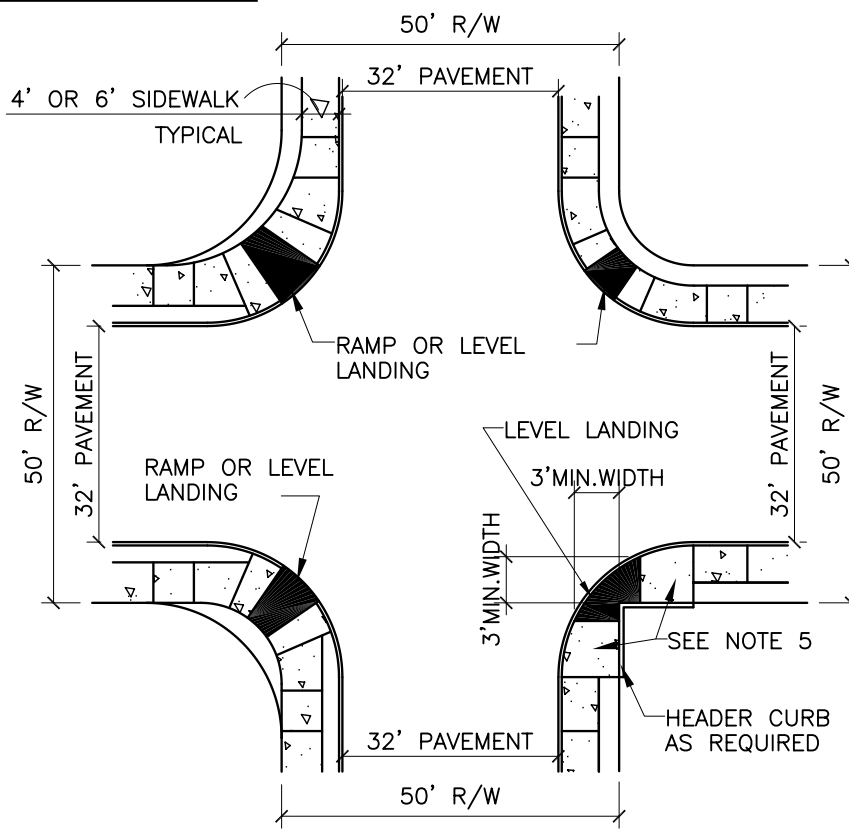
- A. WEAKENED PLANE JOINT ALIGNMENT TO BE RADIAL.
- B.  $\frac{1}{2}$ " EXPANSION JOINT.
- C. WEAKENED PLANE JOINTS SHALL NOT BE GREATER THAN 6 FT. O.C. BETWEEN EXPANSION JOINTS, MEASURED ALONG  $\frac{1}{2}$  OF SIDEWALK.



CURB TYPE WITH VARYING WIDTHS

REVISIONS 11/14/91	CITY OF ALBUQUERQUE
	PAVING SIDEWALK TRANSITIONS DWG. 2432 AUG. 1986

## TYPICAL LOCATIONS OF SIDEWALKS & RAMPS



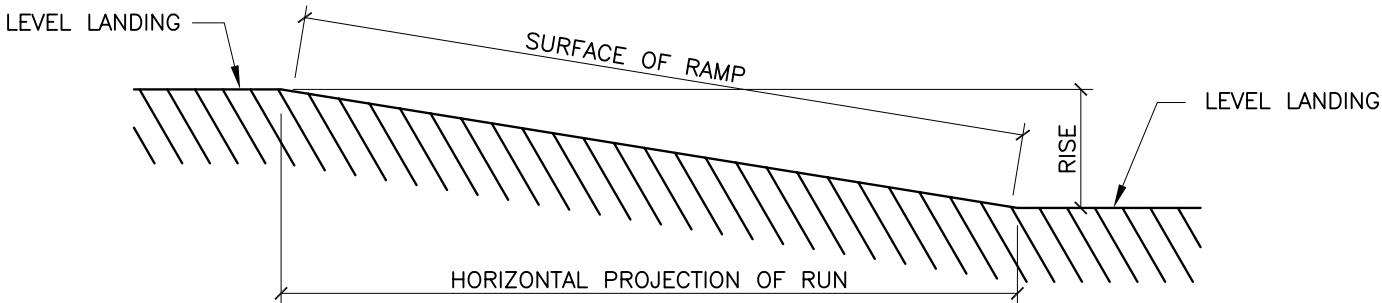
### ADA ACCESSIBLE ROUTE RAMP SLOPES (SEE FIGURE BELOW)

SLOPE *	% SLOPE	MAX. RISE ** INCHES    MM	MAX. HORIZ. PROJ. FEET            METERS	COMMENTS
1:50 or FLATTER	2% OR LESS	UNLIMITED	UNLIMITED	TO BE USED FOR CROSS SLOPES ON ANY INTENDED ADA ACCESSIBLE ROUTE.
1:16 TO 1:20	6.25% TO 5%	30        760	40            12.2	TO BE USED FOR DIRECTION OF TRAVEL ON ANY RAMP SURFACE.
1:12 TO < 1:16	8.33% TO <6.25%	30        760	30            9.1	TO BE USED FOR DIRECTION OF TRAVEL ON ANY RAMP SURFACE.
1:10 TO FLATTER * * *	10% OR LESS	6        150	5            1.5	MAY BE USED AT EXISTING SITES WITH APPROVAL OF THE CITY ENGINEER IF SPACE LIMITATIONS PROHIBIT USE OF A 1:12 SLOPE OR FLATTER.
1:8 OR FLATTER	12.5% OR LESS	3        75	2            0.6	MAY BE USED AT EXISTING SITES WITH APPROVAL OF THE CITY ENGINEER IF SPACE LIMITATIONS PROHIBIT USE OF A 1:12 SLOPE OR FLATTER.

**\*\* AFTER THE MAXIMUM RISE HAS BEEN ATTAINED, A LEVEL LANDING AREA MUST BE PROVIDED.**

\*\*\* SEE GENERAL NOTE NO. 9.

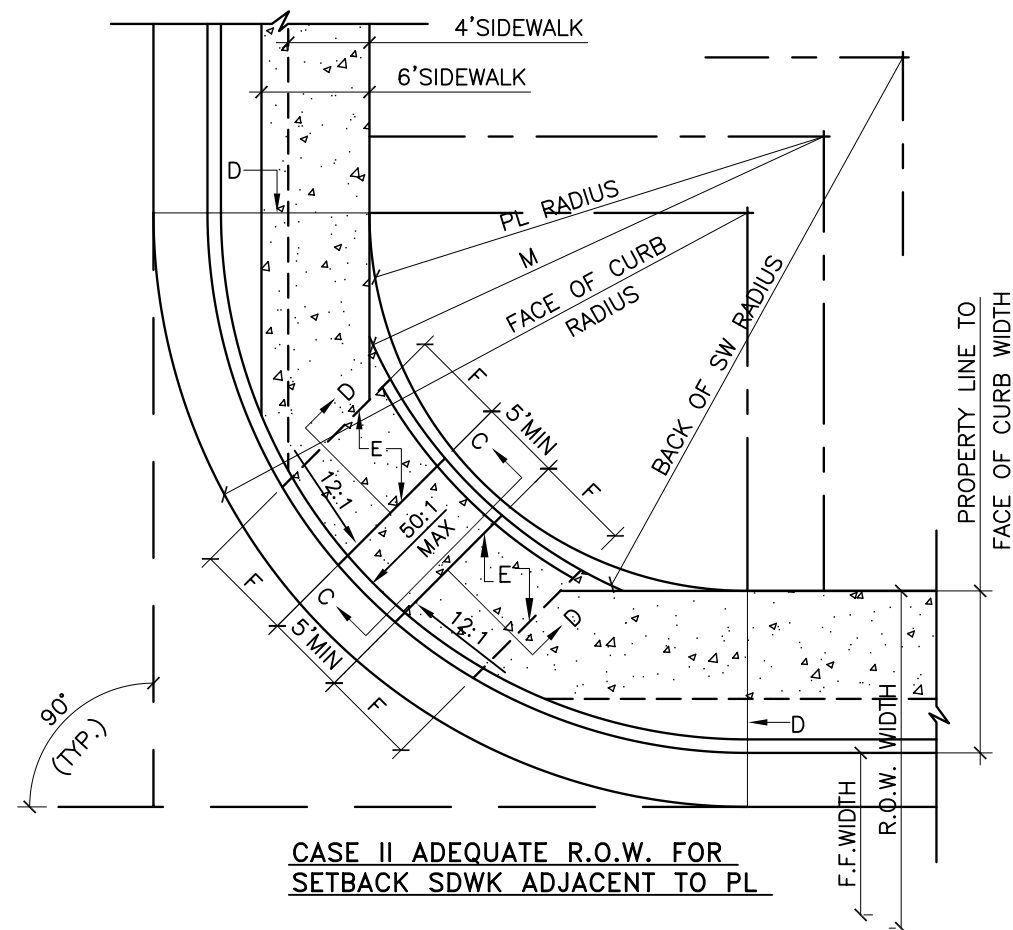
NOTE: ADA DEFINES "RAMP" AS ANY SURFACE THAT EQUALS OR EXCEEDS A 5% SLOPE ALONG ITS PATH OF TRAVEL.  
A LEVEL LANDING AREA IS A SURFACE OF SUFFICIENT SIZE THAT DOES NOT EXCEED A 2% SLOPE IN ANY DIRECTION.



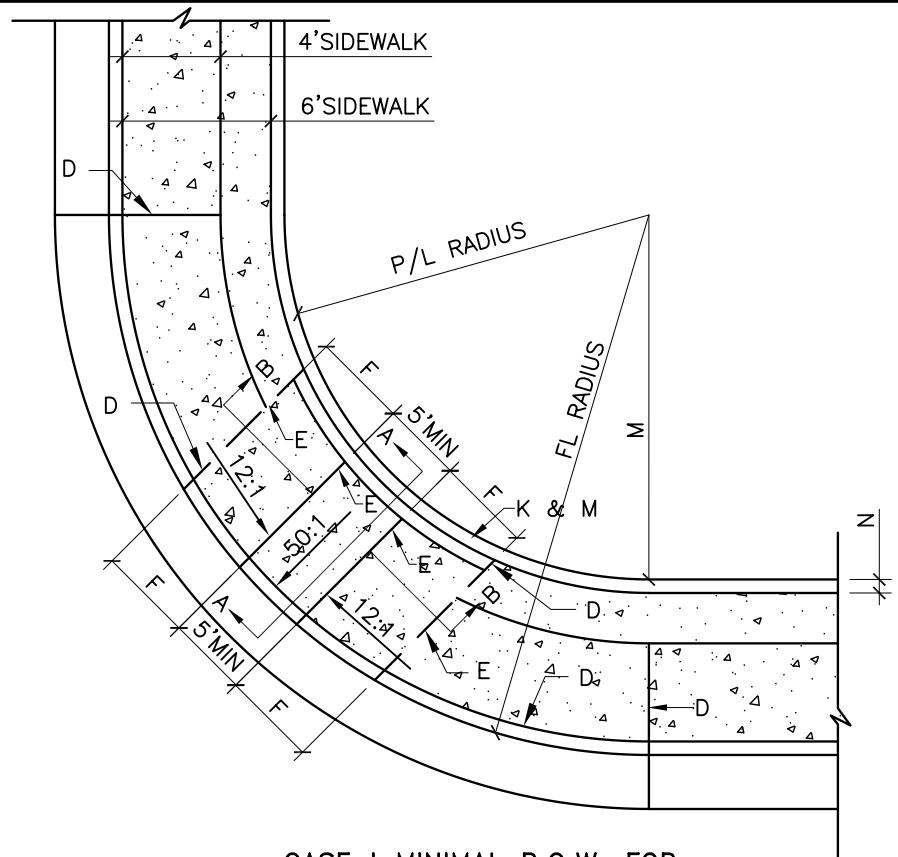
GENERAL NOTES:

1. WHERE AN ADEQUATE AREA CURB ACCESS (WHEELCHAIR) RAMPS EXIST, THE CITY TRAFFIC ENGINEER WILL SPECIFY LOCATION OF RAMPS.
2. MIN. CURB RADIUS IS 25FT. UNLESS OTHERWISE SPECIFIED.
3. CURB ACCESS (WHEELCHAIR RAMPS SHALL BE PROVIDED AT ALL CORNERS OF STREET INTERSECTIONS.
4. SLOPE SIDEWALK FROM TOP OF CURB TO LEVEL LANDING AREA AT BOTTOM OF RAMP ON SLOPE OF 1 (VERTICAL) UNIT TO 12 (HORIZONTAL) UNITS OF IDENTICAL MEASURE (MAXIMUM SLOPE).
5. UNIDIRECTIONAL CURB ACCESS RAMPS: SLOPE SIDEWALK FROM P.C. OR P.T. OF CURB RETURN DOWN TO QUARTER POINT OF CURB RETURN USING A SLOPE NO STEEPER THAN THAT DEFINED IN NOTE 4 ABOVE. FOR POSSIBLE EXCEPTIONS, SEE TABLE OF ADA ACCESSIBLE ROUTE SLOPES ON THIS DRAWING.
6. CURB ACCESS RAMPS COMPLYING WITH ADA REGULATIONS AND THESE DRAWING (2415, 2418, 2425, 2428, & 2441) SHALL BE PROVIDED WHEREVER AN ACCESSIBLE ROUTE CROSSES A CURB.
7. SLOPES OF CURB ACCESS RAMPS SHALL COMPLY WITH ALL ADA REGULATIONS AND THE TABLE OF ACCESSIBLE ROUTE SLOPES OF THIS DRAWING. MAXIMUM SLOPES OF ADJOINING GUTTERS, ROAD SURFACES OR SIDEWALKS ADJACENT TO CURB ACCESS RAMPS SHALL NOT EXCEED 1:20.
8. THE MINIMUM WIDTH OF ANY ADA ACCESSIBLE RAMP SHALL BE 60 IN. (5 FT.).
9. A CURB ACCESS RAMP LOCATED WHERE PEDESTRIANS MUST WALK ACROSS THE RAMP OR WHERE IT IS NOT PROTECTED BY HAND OR GUARDRAIL, SHALL HAVE FLARED SIDES WITH SLOPES NOT EXCEEDING 1:12. IF A LEVEL LANDING AREA OF AT LEAST 48 INCHES LONG IS PROVIDED AT THE TOP END OF THE RAMP. (SEE DWG. 2441, SEC. C-C). OTHERWISE THE FLARED SIDE SLOPES SHALL NOT EXCEED 1:12.
10. CURB ACCESS RAMPS WITH RETURNS OR HEADER TYPE CURBING MAY BE CONSTRUCTED WHERE PEDESTRIANS WOULD NOT NORMALLY WALK ACROSS THE RAMP. BUILT-UP CURB ACCESS RAMPS SHALL BE LOCATED SO THAT THEY DO NOT PROJECT INTO VEHICLE TRAFFIC LANES AND MAY ONLY BE USED WITH APPROVAL FROM THE CITY ENGINEER EXCEPT FOR PARKING LOT APPLICATIONS.
11. CURB ACCESS RAMPS SHALL BE LOCATED OR PROTECTED TO PREVENT THEIR OBSTRUCTION BY PARKED VEHICLES.
12. CURB ACCESS RAMPS AT MARKED CROSSING SHALL BE WHOLLY CONTAINED WITHIN THE MARKINGS EXCLUDING ANY FLARES SIDES.
13. ADA - AMERICAN WITH DISABILITIES ACT.
14. CURB ACCESS RAMPS AND THEIR APPROACHES SHALL BE CONSTRUCTED SO THAT WATER WILL NOT ACCUMULATE ON WALKING SURFACES.
15. ANY CONFLICT BETWEEN COA STANDARD DRAWING AND ADA REGULATIONS SHALL BE BROUGHT TO THE ATTENTION OF CITY ENGINEER FOR RESOLUTION.
16. ALL ADA ACCESSIBLE RAMPS SHALL HAVE LANDINGS AT BOTTOM AND TOP OF EACH RAMP AND EACH RAMP RUN. LANDING SHALL BE AT LEAST AS WIDE AS THE RAMP RUN LEADING TO IT AND SHALL HAVE A LENGTH OF 60 INCHES (5 FT.) MINIMUM. IF THE RAMP CHANGES DIRECTION AT THE LANDING, THE MINIMUM LANDING SIZE SHALL BE 5 FEET BY 5 FEET. RAMPS AND LANDINGS WITH DROP -OFFS SHALL HAVE CURBS, WALLS, RAILINGS, OR PROJECTIONS THAT PREVENTS SLOPPING OR FALLING OFF OF THE RAMP.

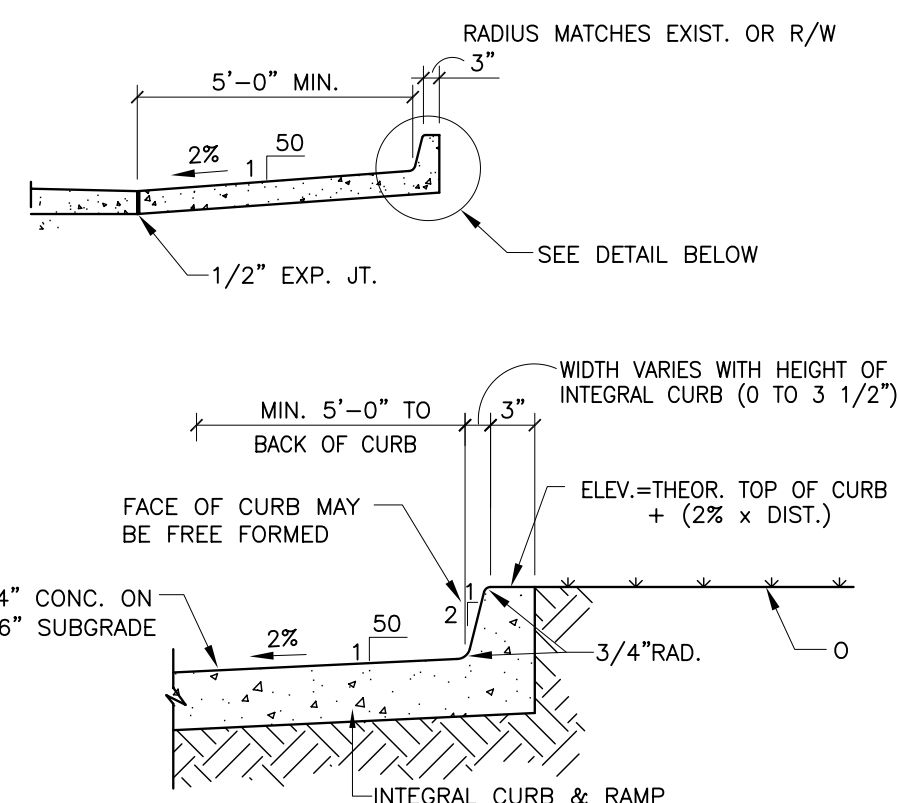
REVISIONS	CITY OF ALBUQUERQUE
11/14/91 4/12/94	PAVING CURB ACCESS RAMP
	DWG. 2440 JANUARY 2003



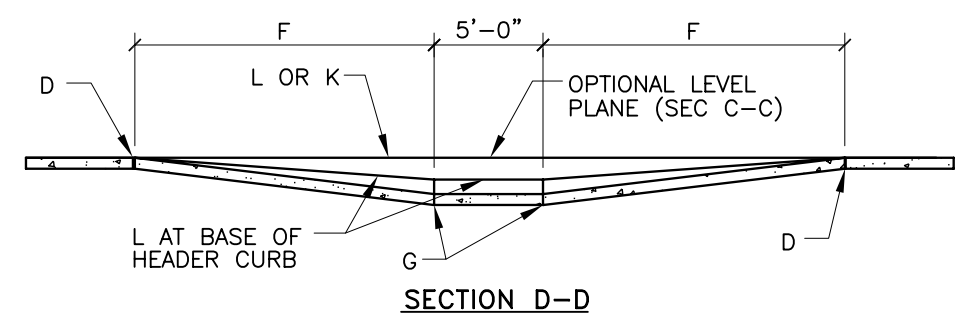
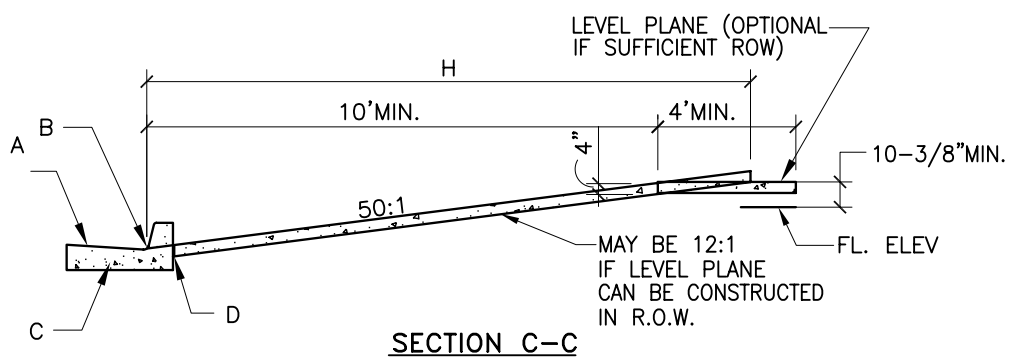
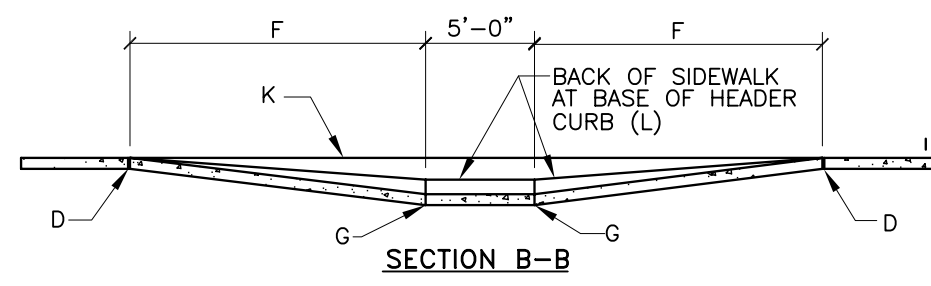
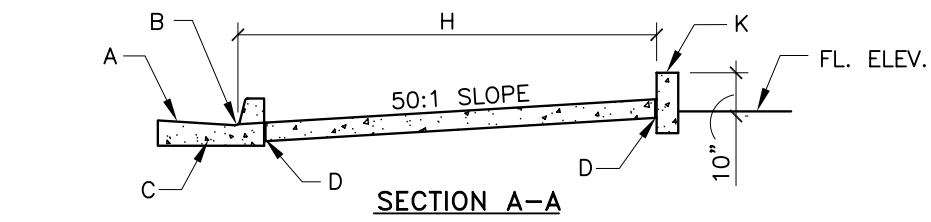
**CASE II ADEQUATE R.O.W. FOR SETBACK SDWK ADJACENT TO PL**



**CASE I MINIMAL R.O.W. FOR SIDEWALK AT BACK OF CURB**



**ALTERNATE SECTION A-A**



**GENERAL NOTES:**

1. CURB ACCESS RAMPS ARE NORMALLY TO BE LOCATED AT THE CENTER OF THE RETURN OR AS DIRECTED BY THE CITY TRAFFIC ENGINEER.
2. SURFACE TEXTURE OF CURB ACCESS RAMPS SHALL BE OBTAINED BY HEAVY BROOMING (TEXTURE DEPTH .0625"), TRAVERSE TO THE SLOPE OF THE RAMP.
3. GUTTER FLOW-LINE PROFILE SHALL BE MAINTAINED THROUGH THE AREA OF THE RAMP. DRAINAGE CATCH BASIN STRUCTURES SHALL NOT BE PLACED IN LINE WITH RAMPS.
4. WIDTH OF SIDEWALK AND RAMP MUST BE MAINTAINED AT A MINIMUM OF 5'-0" THROUGH ENTIRE RAMP LENGTH.

**CONSTRUCTION NOTES:**

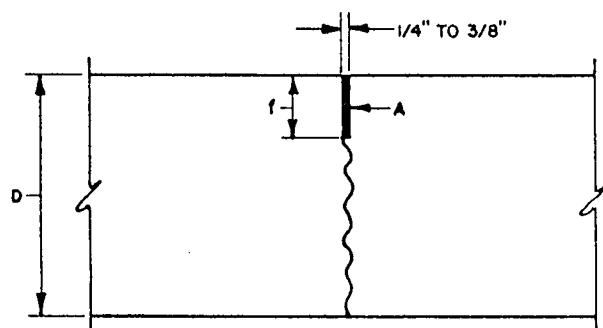
- A. SLOPE OF GUTTER DEPENDENT ON REQUIREMENTS FOR VALLEY GUTTER.
- B. FLUSH WITH RAMP AND GUTTER.
- C. CURB & GUTTER (SEE STD. DWG. 2415-GUTTER AT CURB ACCESS RAMP).
- D. 1/2" EXPANSION JOINT.
- E. PARALLEL LINES-TOP AND BOTTOM OF RAMP.
- F. 12:1 MAX SLOPE OF RAMP.
- G. CONTRACTION JOINT.
- H. VARIES WITH AVAILABLE R.O.W.
- J. VARIABLE.
- K. HEADER CURB, SEE DWG. 2415.
- L. BACK OF SIDEWALK.
- M. BACK OF SIDEWALK RADIUS TO BE ESTABLISHED 90 AS TO MAINTAIN A 5'-0" RAMP WIDTH (MINIMUM) OR NONE THROUGHOUT. SEE STD. DWG. 2440 (NOTE 5) IF LESS THAN 5'-0" IS AVAILABLE DUE TO UNTIMELY UNRESOLVABLE CONSTRAINTS.
- N. 4-1/2" MAX.
- O. ANY PRIVATE LANDSCAPING AND OR IRRIGATION SHALL BE RESTORED TO ORIGINAL CONDITION. SHOULD ANY PRIVATE IMPROVEMENT NEED TO BE REMOVED, OWNER MUST BE NOTIFIED.

△	PL RAD.	CURB RAD.	PL CURB WIDTH	BACK S.W. RADIUS
90°	25'	25'	4'	25'
90°	30'	25'	4'	30'
90°	25'	30'	4'	25'
90°	30'	30'	4'	30'

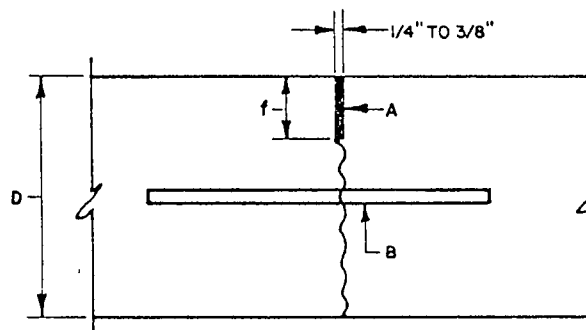
REVISIONS	CITY OF ALBUQUERQUE
11/14/91	PAVING
4/27/94	(WHEELCHAIR) CURB ACCESS RAMP
	DWG. 2441 JANUARY 2003



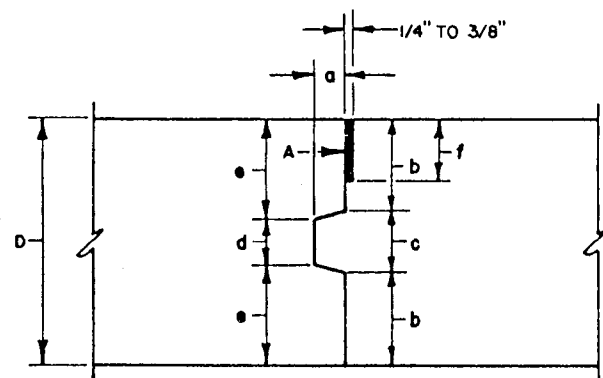
JOINT DIMENSIONS									
						TRANS. JOINTS	LONG. JOINTS		
D	a	b	c	d	e	f - min.	f - min.	g	
5"	1"	1-3/4"	1-1/2"	1"	2"	1/4 D	1/3 D	1/2 D	
6"	1"	2-1/4"	1-1/2"	1"	2-1/2"	1/4 D	1/3 D	1/2 D	
8"	1"	3"	2"	1-1/2"	3-1/4"	1/4 D	1/3 D	1/2 D	



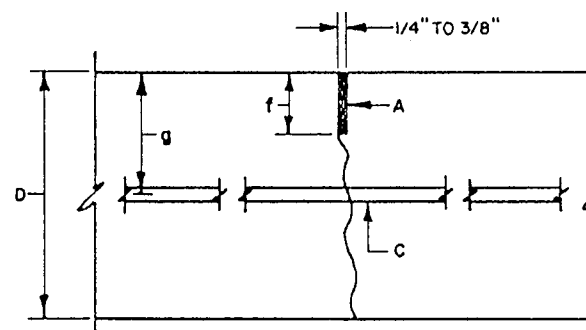
**TYPE 1**  
**SAWED JOINT**  
(LONGITUDINAL OR TRANSVERSE)



**TYPE 2**  
**DOWELED JOINT**  
(TRANSVERSE)  
**ARTERIAL/COLLECTOR**  
**STREETS ONLY**



**TYPE 3**  
**KEYED JOINT**  
(LONGITUDINAL OR TRANSVERSE)



**TYPE 4 ALTERNATE**  
**TIED JOINT**  
(LONGITUDINAL OR TRANSVERSE)

**GENERAL NOTES:**

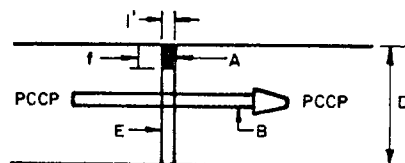
1. THICKNESS OF SLAB SHALL BE AS INDICATED ON DRAWINGS. SEE TABLE ABOVE.
2. DAILY CONCRETE PLACEMENT SHALL TERMINATE AT A JOINT.

**CONSTRUCTION NOTES:**

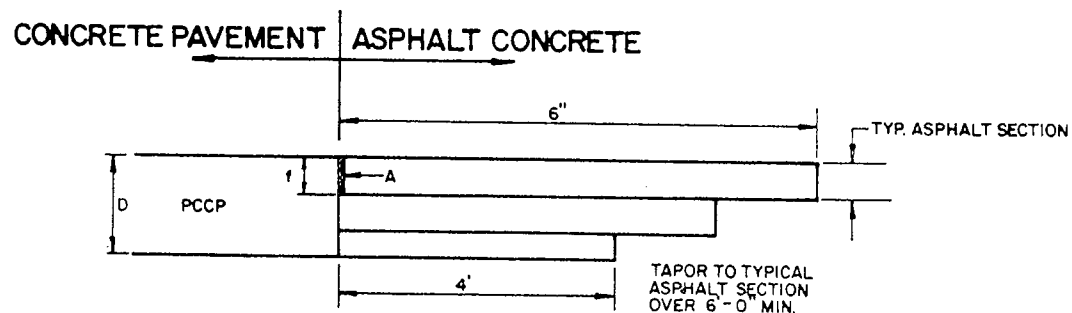
- A. JOINT FILLER, INSTALL PER MANF. INSTR. OVER BACKER ROD OR JOINT TAPE.
- B. 3/4" Ø 16" SMOOTH DOWEL BAR @ 12" O.C., 1/2 GREASED 1/2 PAINTED.
- C. NO. 4 DEFORMED BARS, 3'-0" LONG AT 2'-0" O.C.
- D. THICKNESS OF SLAB.

REVISIONS		CITY OF ALBUQUERQUE	
12/15/92		PAVING	
		CONCRETE JOINTS	
		DWG. 2450	
		AUG. 1986	

JOINT DIMENSIONS								
						TRANS. JOINTS	LONG. JOINTS	
D	a	b	c	d	e	f - min.	f - min.	g
5"	1"	1-3/4"	1-1/2"	1"	2"	1/4 D	1/3 D	1/2 D
6"	1"	2-1/4"	1-1/2"	1"	2-1/2"	1/4 D	1/3 D	1/2 D
8"	1"	3"	2"	1-1/2"	3-1/4"	1/4 D	1/3 D	1/2 D



TYPE 5 EXPANSION JOINT



TYPE 6 TRANSITION JOINT  
CONCRETE TO ASPHALT

GENERAL NOTES:

1. THICKNESS OF SLAB SHALL BE AS INDICATED ON DRAWINGS. SEE TABLE ABOVE.
2. DAILY CONCRETE PLACEMENT SHALL TERMINATE AT A JOINT.

CONSTRUCTION NOTES:

- A. JOINT FILLER, INSTALL PER MANF. INSTR. OVER BACKER ROD OR JOINT TAPE.
- B. 3/4"  $\phi$  16" SMOOTH DOWEL BAR @ 12" O.C., 1/2 GREASED 1/2 PAINTED.
- C. NO. 4 DEFORMED BARS, 3'-0" LONG AT 2'-0" O.C.
- D. THICKNESS OF SLAB.
- E. COMPRESSIBLE FILLER FULL HEIGHT.

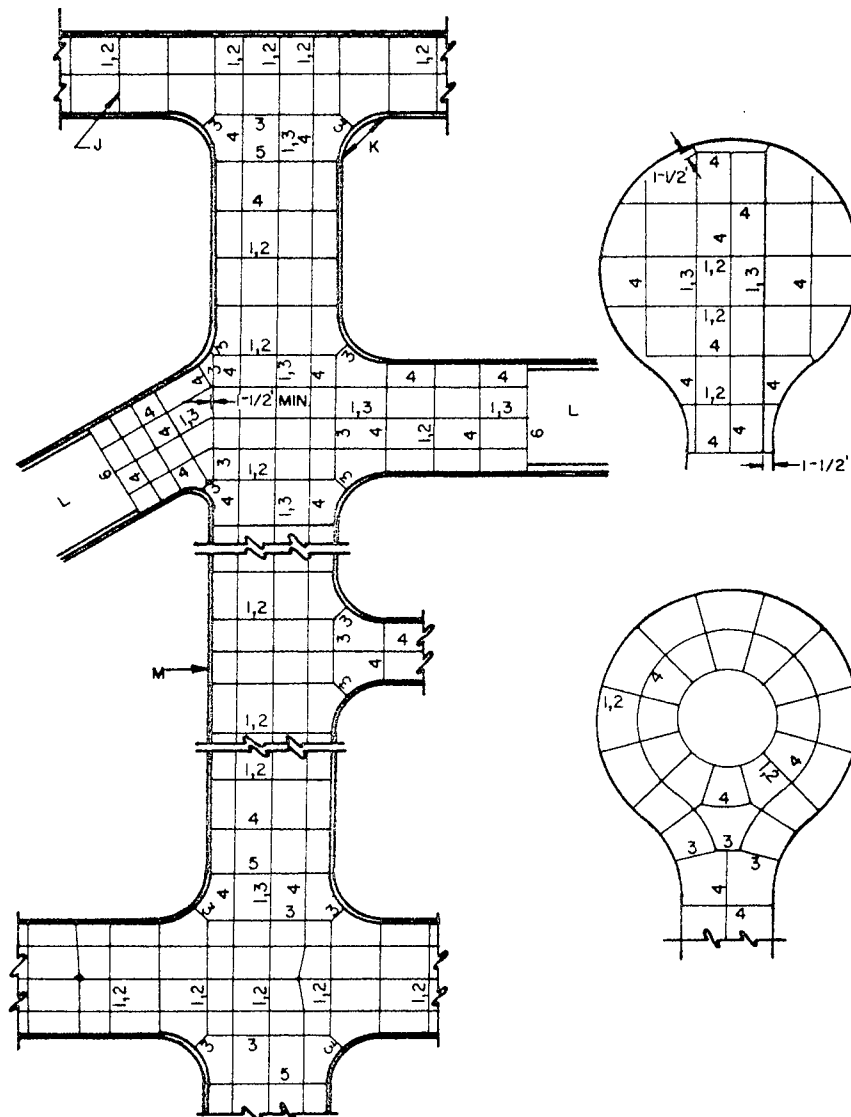
CITY OF ALBUQUERQUE

REVISIONS

PAVING  
CONCRETE JOINTS TYPES 5 & 6

DWG.2451

DEC 1992



TYPICAL CONCRETE PAVEMENT JOINT PATTERN

**GENERAL NOTES:**

1. MAXIMUM DISTANCE BETWEEN JOINTS L, SHALL NOT EXCEED THE FOLLOWING CRITERIA:
  - a. 2.5' PER INCH OF SLAB DEPTH TIMES THE SLAB DEPTH IN INCHES. (2.5'/" X D) WHEN THE SLAB DEPTH IS LESS THAN 10".
  - b. 2' PER INCH OF SLAB DEPTH TIMES THE SLAB DEPTH IN INCHES. (2.0'/" X D) WHEN THE SLAB DEPTH IS 10" OR MORE.
  - c. 15'
2. THE RATIO OF THE LONG SIDE, L, TO THE SHORT SIDE, S, L:S, OF A PAVEMENT PANEL SHALL RANGE FROM 1:1 TO 1.5:1.
3. JOINT LENGTH SHALL NOT BE SHORTER THAN 1.5'.
4. TYPE 2 JOINTS ARE REQUIRED ON COLLECTOR/ARTERIAL STREETS ONLY.

**CONSTRUCTION NOTES:**

- A. THE CONTRACTOR SHALL SUBMIT A PAVEMENT JOINT PLAN TO THE ENGINEER FOR HIS REVIEW AND APPROVAL PRIOR TO THE PLACEMENT OF ANY CONCRETE PAVEMENT. IF THE PLAN DIFFERS FROM THE JOINT PATTERN SHOWN ON THE CONSTRUCTION PLANS, THE CHANGES SHALL BE CALLED OUT. THE JOINT PLAN SHALL INCLUDE THE LOCATIONS OF MANHOLES, VALVE BOXES AND DROP INLETS, AND THE TYPE OF JOINT TO BE CONSTRUCTED. (SEE DWG. 2453).
- B. JOINT 1 MAY BE ALTERNATED WITH JOINT 3 IF THE CONTRACTOR CAN PLACE THREE (3) OR MORE LANES IN A SINGLE PASS.
- C. CONCRETE PLACEMENT SHALL TERMINATE AT A PAVEMENT JOINT.
- D. ALL LONGITUDINAL JOINTS SHALL BE SAWS TO A MINIMUM DEPTH OF D/3.
- E. ALL TRANSVERSE JOINTS SHALL BE SAWS TO A MINIMUM DEPTH OF D/4.
- F. JOINTS SHALL BE SAWS AS SOON AS THE CONCRETE WILL CUT WITHOUT LEAVING A RAVELED EDGE. SAW CUTS SHALL BE CURED SAME AS THE ADJACENT CONCRETE.
- G. ARTERIAL/COLLECTOR PCC PAVEMENT IN INTERSECTIONS SHALL BE FINISHED WITH A TRANSVERSE "RAKE TINED" TEXTURE. THE TINDING SHALL EXTEND A MINIMUM OF 100' AWAY FROM THE INTERSECTION ON THE APPROACHES AND DEPARTURES OF ALL LEGS OF THE INTERSECTION OR THE LENGTH OF THE APPROACH AND DEPARTURE OF THE SIDE STREETS IF LESS THAN 100'.
- H. PCC PAVEMENT BETWEEN INTERSECTIONS AND RESIDENTIAL STREETS/INTERSECTIONS SHALL BE FINISHED WITH A FULL WIDTH LONGITUDINAL COARSE TEXTURE BURLAP DRAG.
- J. END OF DAYS WORK.
- K. PLACE 1/2" EXPANSION JOINT FILLER IN CURB AT ALL RADIUS POINTS.
- L. ASPHALT PAVEMENT.
- M. FIRST STREET PAVED.

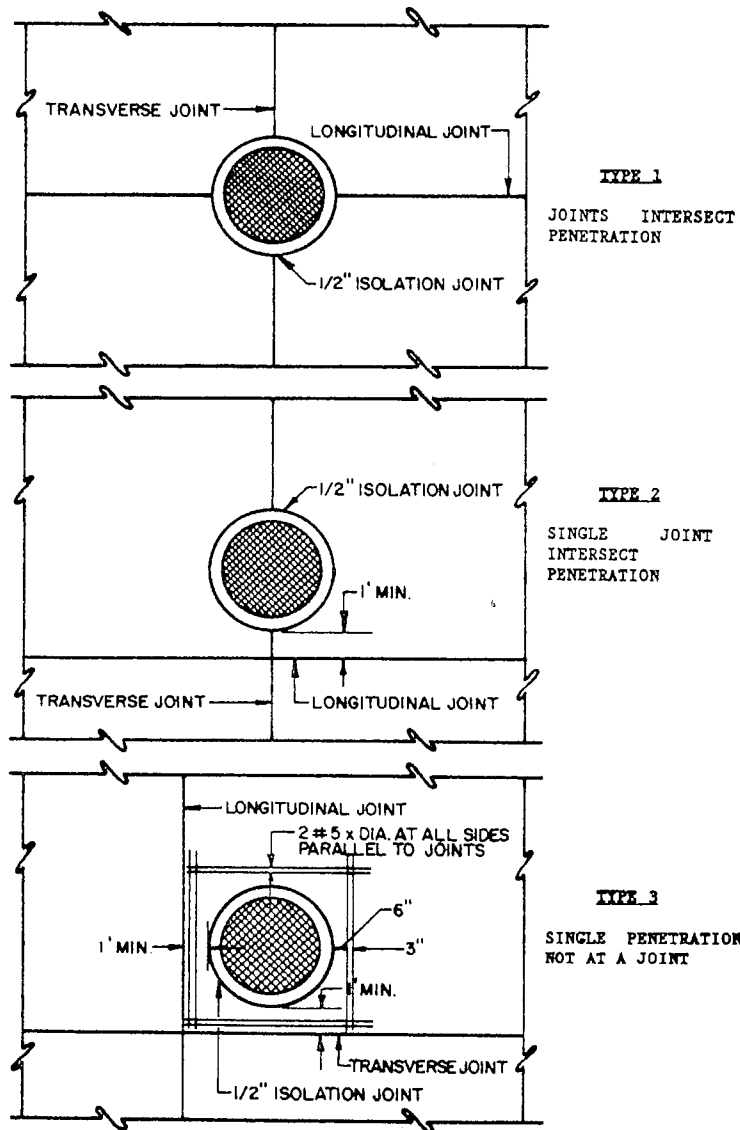
CITY OF ALBUQUERQUE

REVISIONS

PAVING  
TYPICAL CONCRETE PAVEMENT  
JOINT PATTERN

DWG. 2452

DEC. 1992



#### TYPE 4

PANELS WITH 2 (TWO) OR MORE PENETRATIONS IN A SINGLE PANEL, THE PANEL SHALL BE REINFORCED BETWEEN BOTH TRANSVERSE AND LONGITUDINAL JOINTS WITH #5 EACHWAY AT 6" O.C. CONTINUOUS BETWEEN JOINTS

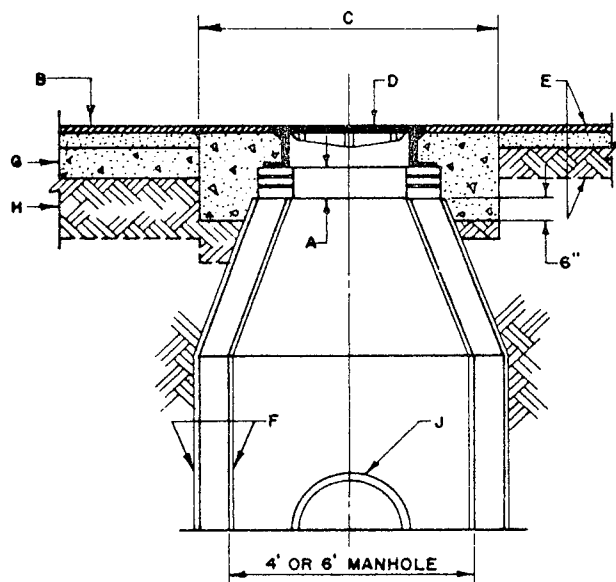
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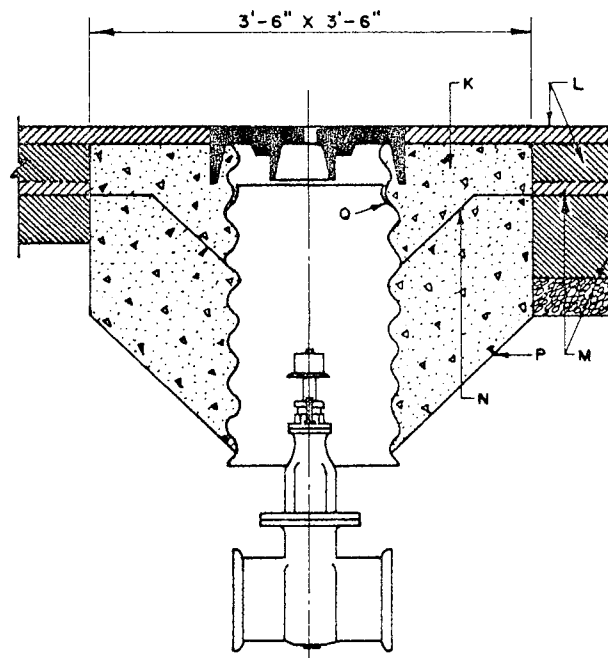
PAVING  
STANDARD PENETRATIONS  
PCC PAVEMENT

DWG. 2453

DEC. 1992



MANHOLE REGRADING DETAIL



VALVE BOX REGRADING DETAIL

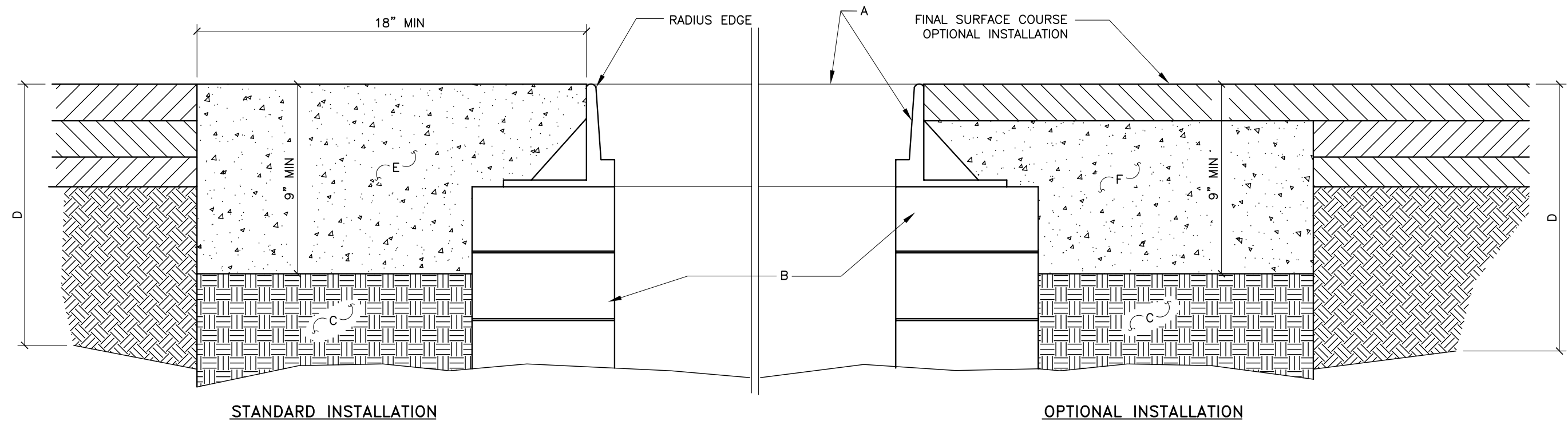
### GENERAL NOTES:

1. ADJUSTMENT TO GRADE OF FRAME AND COVER SHALL BE MADE BY VARYING BRICK COURSES DIRECTLY UNDER FRAME, USING FOUR COURSE MAX. IF LESS THAN ONE COURSE IS REQUIRED, GROUT ONLY SHALL BE USED. IF ADJUSTMENT WOULD REQUIRE GREATER THAN FOUR COURSES, THE CONE SHALL BE REMOVED, BARREL HEIGHT ADJUSTED AND CONE REPLACED. ADJUSTMENT RINGS MAY BE USED FOR MINOR ADJUSTMENT REQUIREMENTS.
2. FOR SMALL ADJUSTMENTS WATER VALVE EXTENSION COLLAR & INSERT MAY BE USED. SEE DWG. 2332.

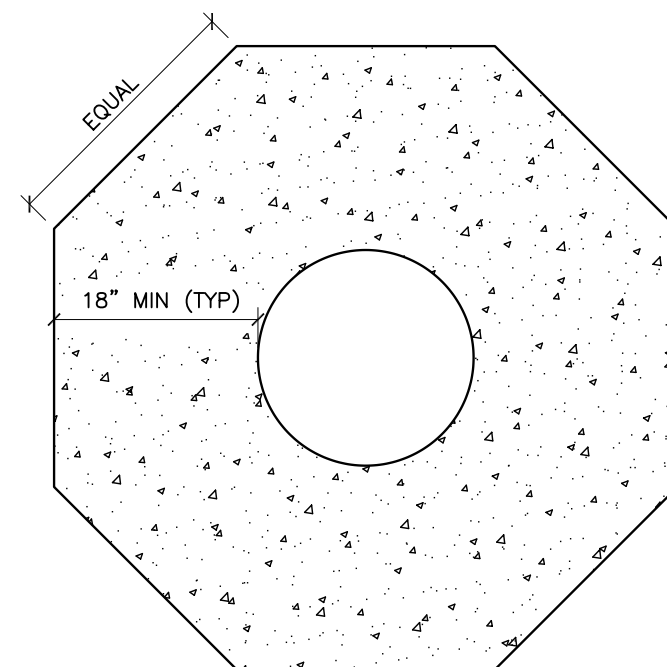
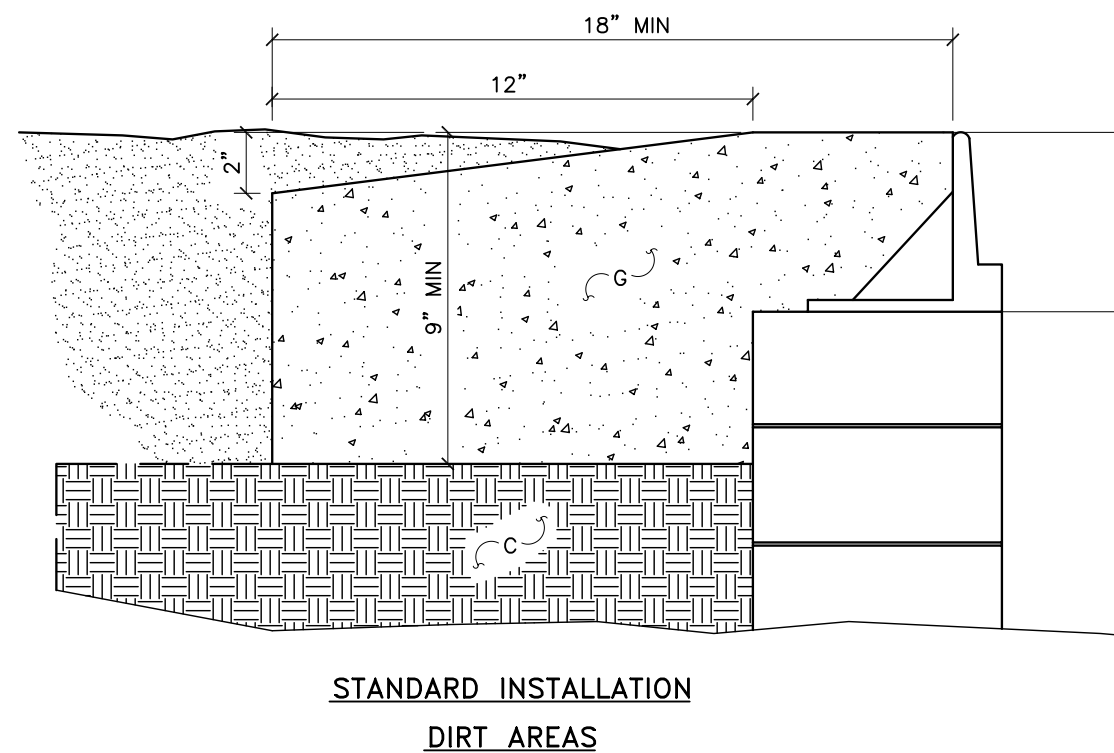
### CONSTRUCTION NOTES:

- A. FOUR BRICK COURSES MAX.
- B. OVERLAY.
- C. USE A CONC. PAD 5' X 5' IN ALL AREAS.
- D. M.H. FRAME & COVER.
- E. STD. PAVING SECT.
- F. 1/2" PLASTER INSIDE & OUTSIDE EXCEPT ON PRE-CAST UNITS.
- G. A.T.B. OR C.T.B.
- H. SUBGRADE.
- J. SEWER LINE.
- K. NEW PORTLAND CEMENT CONC.
- L. NEW PAVING MATERIAL.
- M. EXIST. PAVEMENT.
- N. CUT LINE.
- P. EXIST. CONC.
- O. VALVE BOX EXTENSION, SEE DWG. 2325.

REVISIONS	CITY OF ALBUQUERQUE	
	PAVING	
	MH & VALVE BOX REGRADING	
	DWG. 2460	
		AUG. 1966



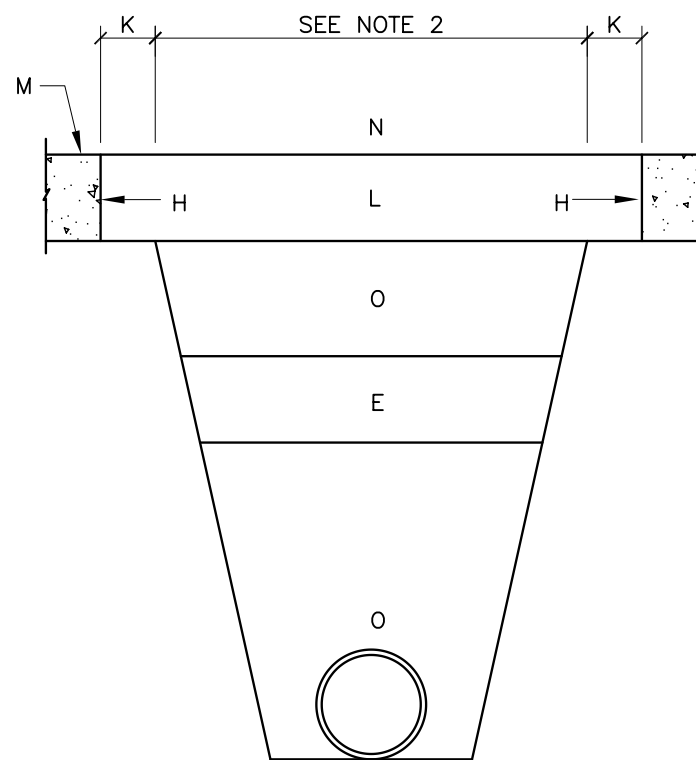
PAVED AREAS



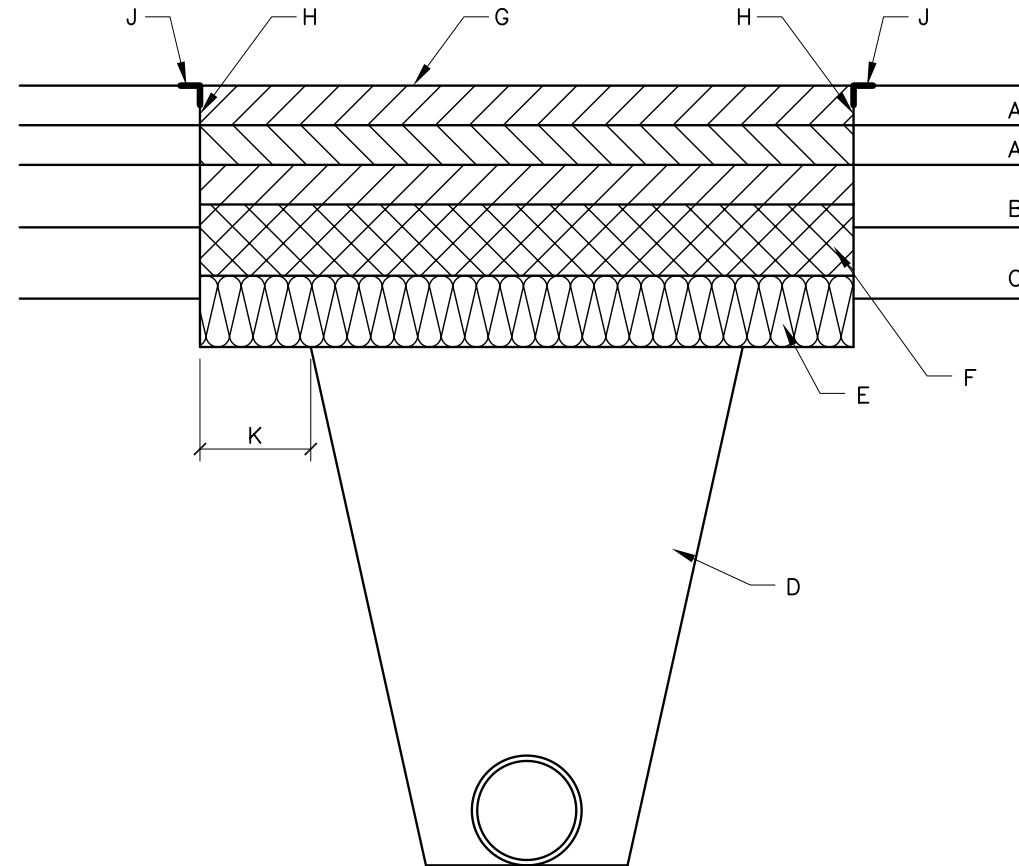
GENERAL NOTES:

- A MANHOLE OR VALVE BOX RING AND COVER PER CITY STANDARDS.
- B MANHOLE CONE/EXTENSION OR VALVE PIPE PER CITY STANDARDS. PIPE WITH SMOOTH INTERIOR.
- C 12" SUBGRADE, 95% COMPACTION (ASTM).
- D PAYING SECTION PER APPROVED DRAWINGS.
- E CONCRETE COLLAR IN PAVED AREAS – TYPICAL INSTALLATION.
- F CONCRETE COLLAR IN PAVED AREAS WITH ASPHALT CAP. TO BE USED WHEN CALLED FOR ON PLANS OR AS DIRECTED BY THE ENGINEER.
- G CONCRETE COLLAR IN DIRT AREAS – SET RING 1" ABOVE GRADE AND SLOPE CONCRETE DOWN AS SHOWN TO 1" BELOW GRADE.
- H WATER VALVE INSTALLATIONS SHALL HAVE SURFACE STAMPED WITH LINE INFORMATION PER CITY STANDARD DWG 2326.

REVISIONS	CITY OF ALBUQUERQUE
	MANHOLE/VALVE CONCRETE COLLAR DETAIL
DWG. 2461	JANUARY 2003



CONCRETE PAVEMENT



ASPHALT CONCRETE PAVEMENT

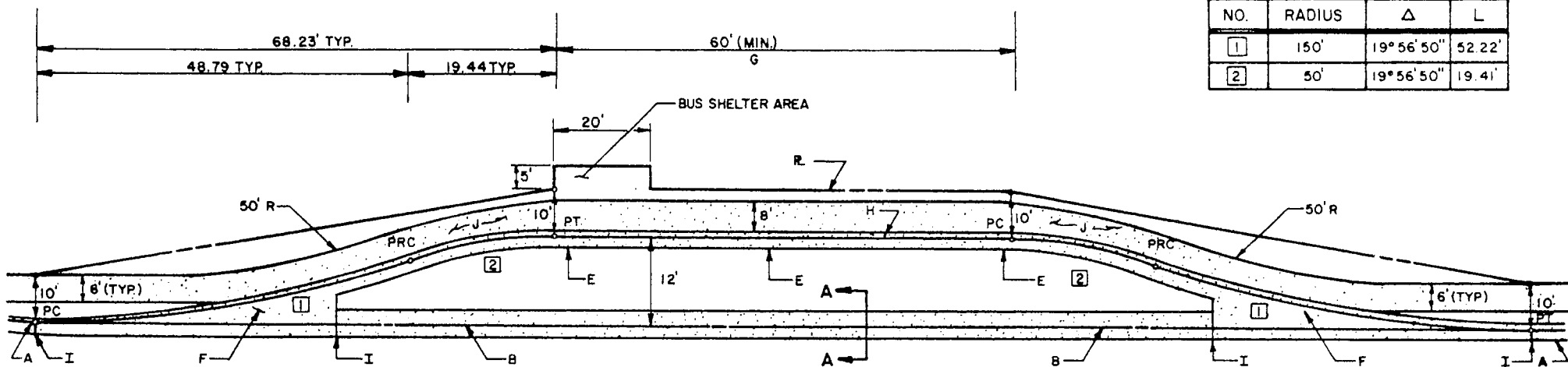
**GENERAL NOTES:**

1. COMPACTION AS DETERMINED BY ASTM D1557 MAX DENSITY.
2. TRENCH CUT WIDTHS SHALL BE MIN. WIDTH REQ'D FOR UTILITY INSTALLATION, ECONOMICAL BACKFILL COMPACTION AND COMPLIANCE WITH CURRENT AND APPLICABLE SAFETY REGULATIONS.
3. ALL PAVEMENTS CUT EDGES WILL BE TRIMMED TO PRESENT AN EVEN LINE PRIOR TO REPLACEMENT OF PAVING MATERIAL "STITCH" CUTTING OF PAVEMENT WILL NOT BE PERMITTED.
4. ADDITIONAL 2" THICKNESS OF ASPHALT CONC. REQ'D ON PAVEMENT CUTS LESS THAN 8' WIDE FOR ASPHALT CONC. PAVEMENT CUTS 8' OR MORE IN WIDTH AND LONGER THAN 100' SHALL BE PLACED WITH LAYDOWN MACHINE TO A DEPTH EQUAL TO THAT OF ASPHALT CONC. REMOVED.

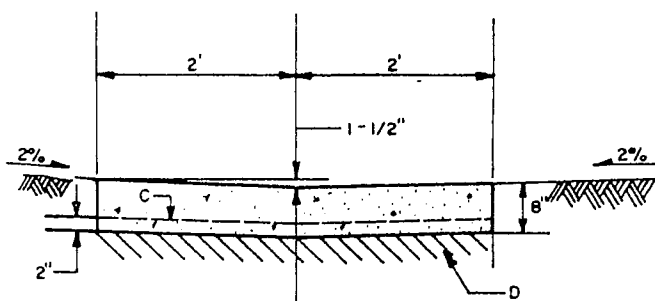
**CONSTRUCTION NOTES:**

- A. EXISTING ASPHALT PAVEMENT.
- B. EXISTING BASE MATERIAL (ABC, BTB, CTB)
- C. EXISTING SUBGRADE
- D. COMPACTED FILL, 95% COMPACTION
- E. COMPACTED SUBGRADE, 95% COMPACTION. SUBGRADE TO MEET OR EXCEED APPARENT R-VALUE OF ADJACENT SOIL, BY SOIL CLASSIFICATION (2 FEET MIN.).
- F. MATCH EXISTING BASE MATERIAL PLUS AN ADDITIONS 2" OF THICKNESS - 95% COMPACTION
- G. MATCH EXISTING ASPHALT CONCRETE SECTION PLUS AN ADDITIONAL 2" OF THICKNESS
  - a) FOR RESIDENTIAL STREETS, SURFACE COURSE SHALL BE 1 1/2" THICK, TYPE C
  - b) FOR MAJOR LOCAL STREETS, SURFACE COURSE SHALL BE 2" THICK, TYPE B
  - c) FOR ALL OTHER STREETS, SURFACE COURSE SHALL BE 2" THICK, S-III
- H. SAW CUT OR BLADE-CUT ASPHALT PAVEMENT. SAW CUT ONLY ONE THIRD CONC. DEPTH
- J. TACK COAT
- K. 12" CUT-BACK
- L. MATCH EXISTING CONCRETE PAVEMENT THICKNESS, 6" MINIMUM, 4000 PSI
- M. EXISTING CONCRETE PAVEMENT
- N. JOINTS TO BE TOOLED & SEALED IN ACCORDANCE WITH ENGINEERS REQUIREMENTS
- O. 6" CONC. TREATED BASE (C.T.B).

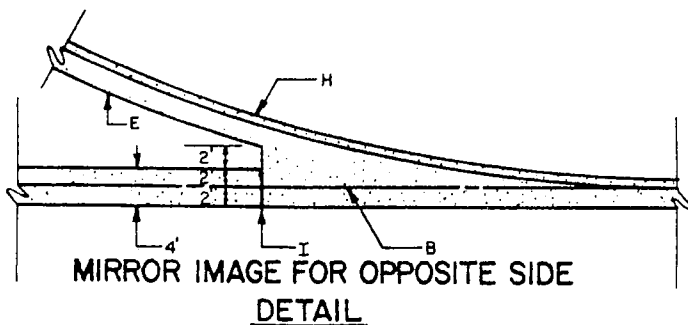
REVISIONS	CITY OF ALBUQUERQUE
1/91	PAVING
12/15/92	CITYWIDE PAVEMENT CUTS FOR ALL UTILITIES
	DWG. 2465 JANUARY 2003



NO.	RADIUS	$\Delta$	L
1	150'	19° 56' 50"	52.22'
2	50'	19° 56' 50"	19.41'



SECTION A-A



#### GENERAL NOTES

- DESIGN ELEVATIONS TO BE GIVEN AT PC'S, PRC'S, AND PT'S OF CURB CURVES AND AT 50' MINIMUM INTERVALS AT VALLEY GUTTER INVERT.
- THE INVERT OF THE VALLEY GUTTER TO EXTEND FROM THE FLOWLINE OF THE UPSTREAM PC TO THE FLOWLINE OF THE DOWNSTREAM PT CONCENTRIC TO THE CENTERLINE.
- THE VALLEY GUTTER TO BE REINFORCED WITH 6" X 6" X NO. 6 GA. WIRE MESH.
- FOR NEW CONSTRUCTION, VALLEY GUTTER SHALL BE CONSTRUCTED PRIOR TO ADJACENT PAVEMENT. ASPHALT CONCRETE SHALL BE INSTALLED MONOLITHICALLY TO MEET NEW VALLEY GUTTER.
- PRIOR TO CONSTRUCTION OF NEW VALLEY GUTTER ON EXISTING ACCEPTED STREETS, PAVEMENT SHALL BE REMOVED AS SHOWN ON PLANS.
- LOCATE EXPANSION AND CONTRACTION JOINTS AS PER CITY STANDARD DRAWING NO. 2415.
- BUS SHELTER AREA IS OPTIONAL. BUS SHELTER TYPE "A" SEE 2533.1 - 2533.14. BUS SHELTER TYPE "B" SEE 2534.1 - 2534.12.

#### CONSTRUCTION NOTES

- TANGENT - SEE CITY STANDARD DRAWING NO. 2415.
- FLOWLINE.
- 6" X 6" X NO. 6 GA. WIRE MESH.
- FOUNDATION FOR VALLEY GUTTER SHALL BE EQUAL TO BASE, SUBBASE AND SUBGRADE REQUIREMENTS FOR ADJACENT PAVEMENT SECTION BELOW BOTTOM OF GUTTER.
- SLOPE PAVING TO VALLEY GUTTER. PAVEMENT MAYBE ASPHALT OR CONCRETE.
- SURFACE AND CURB TO BE MONOLITHIC.
- LENGTH TO BE DETERMINED BY CITY OF ALBUQUERQUE TRAFFIC ENGINEER.
- DEPRESSED GUTTER - SEE CITY STANDARD DRAWING NO. 2415.
- 1/2" EXPANSION MATERIAL.
- 6' MINIMUM SIDEWALK.

CITY OF ALBUQUERQUE

REVISIONS

PAVING  
BUS BAY

DWG.2466

JUNE 1991